



ST PHILIPS

Define.

# LAND NORTH OF PENKRIDGE

**DESIGN & ACCESS STATEMENT:**  
Masterplan, Placemaking Code &  
Green Infrastructure Strategy

NOVEMBER 2023

# STUDENT CONCO





## 6 SUSTAINABILITY

121

## 8 GREEN INFRASTRUCTURE CODE

95

## 7 THE PLACEMAKING CODE

59

## 9 MASTERPLAN PRINCIPLES

47

## 5 THE MASTERPLAN

37

## 4 THE VISION

31

## 3 UNDERSTANDING THE SITE & CONTEXT

13

## 2 POLICY CONTEXT

9

## 1 INTRODUCTION

5

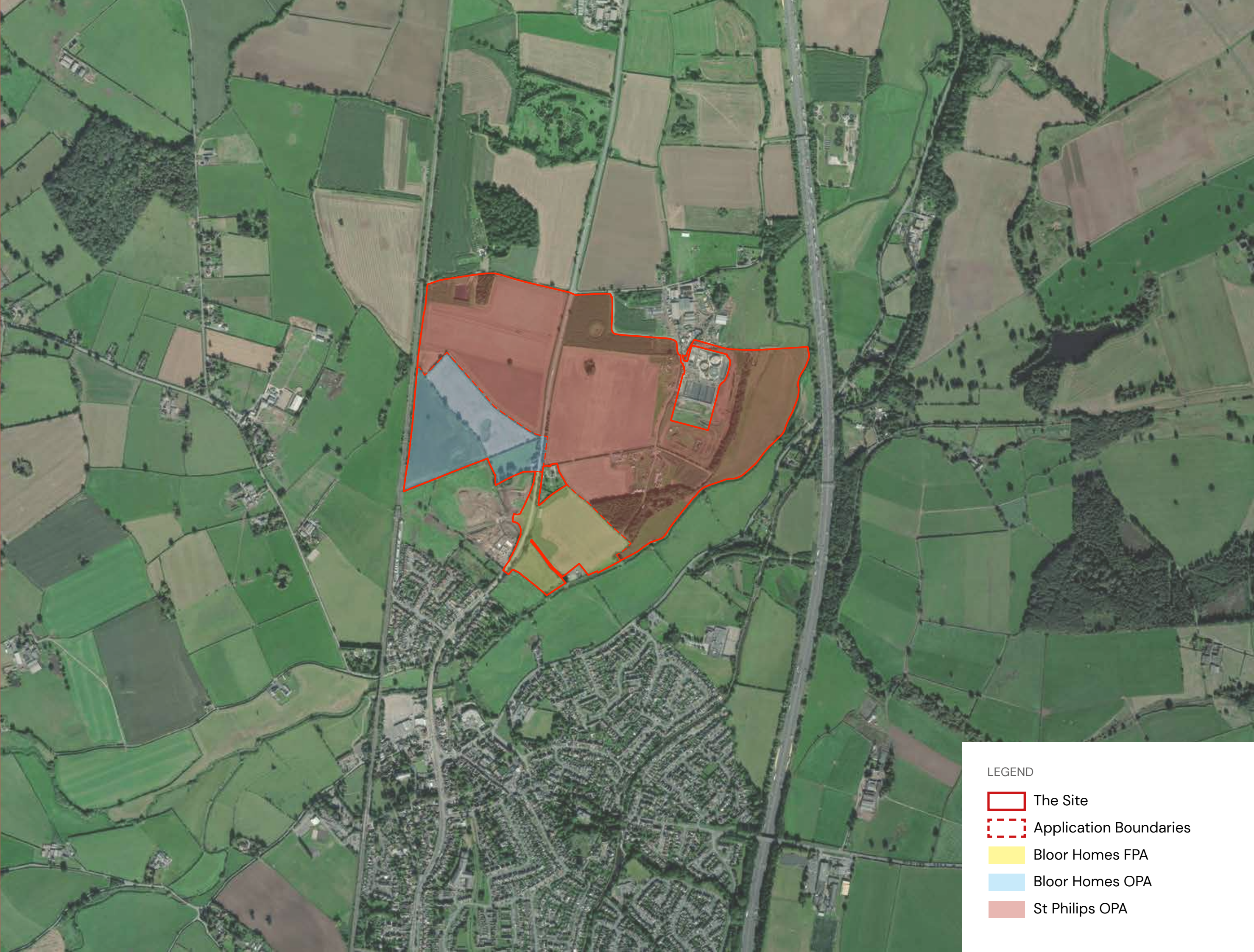


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

# INTRODUCTION



Site Location



LEGEND

-  The Site
-  Application Boundaries
-  Bloor Homes FPA
-  Bloor Homes OPA
-  St Philips OPA



# Introduction

## PURPOSE

This Design and Access Statement (DAS) has been prepared on behalf of St Philips to support their outline planning application for the development of 'Land North of Penkridge' (Figure 1).

The application site is part of proposed strategic housing allocation site SA4 as identified by South Staffordshire Council (SSC) in their emerging Local Plan (eLP), herein referred to as The Site.

The application is one of three applications for the main part of the allocation site. The three applications relate to this outline planning application (OPA) for the land controlled by St Philips at the north of The Site, an OPA for the land that is controlled by Bloor Homes to the west of the A449, and a Full Planning Application (FPA) for the land controlled by Bloor Homes to the east of the A449. The delineation between the applications is shown on the adjacent plan.

This DAS summarises the process undertaken in advancing the Strategic Masterplan for The Site's development. It presents the context for the development, the high-level vision and the key design drivers, and establishes a clear framework for the comprehensive and cohesive development of The Site on that basis. It also presents detailed placemaking and landscape coding principles that will be implemented to realise a high-quality and cohesive new community to the north of Penkridge.

## THE SITE

The Site is located to the north of Penkridge and largely comprises agricultural land, with some related uses dispersed throughout The Site.

The northern boundary of The Site is delineated by a strategic bridleway to the west of the A449 and Lower Drayton Lane to its east, the eastern boundary is marked by the River Penk's western bank, and The Site's western boundary is delineated by the West Coast Mainline. Land to the south of The Site comprises the northern extents of Penkridge village.

## THE PROPOSAL

The development of The Site will collectively deliver the following:

*Residential-led mixed use development comprising up to 1,100 dwellings including a minimum of 40 units of Specialist Older People's Housing (Use Class C2 and / or C3), a site for the delivery of a first school and nursery (Use Class F1 and E(f)), a Community Hub of 1,000m2 floorspace incorporating convenience retail, commercial premises and / or work space, food takeaway / drinking establishment, community space (if required), a mobility hub, public realm and residential uses (Use Classes E, F2, Sui Generis, C3), a Community Park, and associated access, drainage, green and blue infrastructure (including sports, public open space, planting and landscaping), ground remodelling and ancillary infrastructure (including electric substations and foul pumping stations), as well as required demolition of existing structures.*

## THE PROPOSAL

This document is structured as follows:

- Section 2 – Policy Context: Provides the context in relation to the site allocation policy and its requirements, and sets out national and local policies that will inform the design response;
- Section 3 – Understanding the Site and Context: Summarises the site's context relating to the built form and the surrounding landscape, and sets out the key site opportunities and constraints;
- Section 4 – The Vision: Establishes the aspiration for the development of the site, focusing on key development parameters;
- Section 5 – The Masterplan for Development: Presents the overarching Masterplan for The Site's development;
- Section 6 – Masterplan Principles: Outlines the key principles that have informed the Strategic Masterplan, and the justification for the design solutions that have been identified;
- Section 7 – The Placemaking Code: Sets out the design aspiration, and prescribes the detailed design response for specific places and spaces;
- Section 8 – The Green Infrastructure Code: Sets out Green and Blue Infrastructure, landscape and tree planting coding principles;
- Section 9 – Sustainability: provides an overview of the sustainable credentials that are embedded in the Masterplan;
- Section 10 – Other Matters: Provides further detail in relation to detailed considerations.



2

# POLICY CONTEXT

# Policy Context

## THE DEVELOPMENT PLAN

The extant Development Plan comprises the Core Strategy (CS) and Site Allocations Document (SAD) that set the vision and spatial planning framework for the area, as well as a number of Supplementary Planning Documents (SPDs).

The CS is, however, out of date in NPPF terms and SSC is therefore in the process of reviewing its local plan, having consulted on the Regulation 19 version of the eLP in December 2022. Once adopted, the eLP will replace the CS and SAD.

Both the extant local plan and eLP identify Penkrige as a Tier 1 settlement within the settlement hierarchy, reflecting that it contains a wide range of services and facilities and is well-connected to higher order settlements. Both recognise the role that Penkrige can play in accommodating future growth.

## LOCAL PLAN ALLOCATION

In that context, eLP Policy SA4 identifies a strategic housing allocation at the north of Penkrige for the delivery of 1,129 dwellings and supporting infrastructure. The Site forms the vast majority of that proposed allocation, save for a small area to its south that is controlled by Trine Developments.

eLP Policy SA4 sets out the following requirements for development at the north of Penkrige:

- A minimum of 1,129 homes including affordable housing and special elderly housing;
- A well located and connected community hub

including a new first school, local convenience retail, other commercial premises and flexible community space;

- A community park;
- Vehicular access from the A449 with a village gateway and public transport provision;
- Sports provision;
- Active travel infrastructure within and beyond the site, notably to the village centre;
- Integrated and connected green and blue infrastructure;
- Any necessary historic environment mitigation; and
- Contributions as necessary to off-site highway and community infrastructure.

Policy MA1 also sets out policy requirements relevant to strategic housing allocations.

## DESIGN POLICY AND GUIDANCE

### National Planning Policy and Guidance

#### *The National Planning Policy Framework (NPPF)*

The NPPF is underpinned by the presumption in favour of sustainable development. Paragraph 126 states that the creation of high quality building and places is fundamental to what the planning / development process should achieve, and that good design is a key aspect of sustainable development.

Paragraph 119 states that planning decisions should promote an effective use of land in meeting the need for homes and other uses.

Paragraphs 132 – 135 emphasise the importance of design quality through the evolution and assessment of development proposals.

Paragraph 92 highlights that the planning system has an important role in achieving healthy, inclusive and safe places that promote social interaction, and enable and support healthy lifestyles.

Paragraph 104 states that transport issues should be considered at the earliest possible stage when evolving development proposals to ensure that the potential impacts of development on transport can be addressed. It states that patterns of movement, streets, parking and other transport considerations are integral to the design of schemes and contribute to making high quality places.

Paragraph 105 adds that significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes.

#### *The National Design Guide (NDG):*

The NDG illustrates how well-designed places that are beautiful, enduring and successful can be achieved in practice. The NDG is designed to be read alongside the NPPF.

#### *The National Model Design Code (NMDC):*

The NMDC provides detailed guidance in relation to the production of design codes, guides and policies to promote successful design.



## Local Planning Policy and Guidance

CS Core Policy 4 states that all new developments must achieve a high quality of design by meeting the following requirements:

- To provide an attractive, functional, accessible, safe, healthy and secure environment;
- To respect and enhance local character and distinctiveness of the natural and built environment including opportunities to improve the character and quality of the area and the way it functions;
- To incorporate measures to reduce the risk of flooding and prepare for the predicted effects of climate change;
- To make a positive contribution to the public realm including the incorporation of public art where appropriate;
- To adopt sustainable construction principles and use locally sourced and recycled materials wherever possible;
- To incorporate accessibility measures to meet the needs of users and facilitate access through sustainable forms of transport;
- To facilitate and encourage physical activity through outdoor sport, recreation (informal sport and play) walking and cycling;
- To secure improvements to public spaces and the provision of additional public spaces.

CS Policy EQ11 states that developments must be of the highest quality and requires the submission of design statements that explain proposals.

CS Policy EQ12 states that landscape schemes must be an integral part of the overall design of new developments, and should complement and enhance the development and wider area.

The adopted Design Guide SPD (2018) also provides additional guidance to help applicants understand the Council's requirements for the design of new development in the District.

eLP Policy HC10 effectively combines the above policies, requiring developments to achieve creative and sustainable design from the outset of the development and through its lifetime.

There are also a number of other policies within the extant and emerging plans that influence design. A summary of the relevant policies is appended to the Planning Statement, which itself assesses whether the proposals comply with relevant policies.





# UNDERSTANDING THE SITE & CONTEXT

# The Site & Context

The Site is an entirely appropriate and sustainable location for growth. This has been established in the eLP's evidence base and the site assessments, which have provided a robust understanding of The Site.

## PENKRIDGE

Penkridge is located between Stafford to its north and Wolverhampton to its south. The majority of the settlement lies on the south and east bank of the River Penk. As a settlement, Penkridge has generally grown southwards, with the settlement contained to the east by the M6 and to the west by the railway line. Beyond the M6 to the east lies Cannock Chase, approximately 4km from Penkridge.

The centre of Penkridge lies c. 1km to the south of The Site and is mainly focussed around Stafford Road and Market Street. There is a wide range of services and facilities on offer within Penkridge village centre in close proximity to The Site.

This includes a nursery, several schools (first, middle and high schools), retail stores / supermarkets, a community centre, library, GP surgery, dental surgery, and places of worship. The village also benefits from a range of leisure facilities, including sports and recreation centres, public parks (incorporating play areas and sports pitches), a range of non-convenience retail shops within the village centre, and numerous cafes, restaurants, pubs and bars.

## ACCESS AND MOVEMENT

The Site is well-connected with the existing village and the services and facilities therein.

The Site is bisected by the A449 Stafford Road, which connects Penkridge with Stafford, Wolverhampton, Kidderminster and Worcester and forms part of the Strategic Road Network.

Northbound and southbound bus stops are located along the A449, near to the junction with Lower Drayton Lane at The Site's northern boundary, to the north of the recently constructed roundabout, and immediately adjacent to the southern boundary of The Site. The stops are served by the 875, 878 and 879 services and collectively provide services between Penkridge village centre, Stafford, Rodbaston (including Rodbaston College), Cannock, Wolverhampton and a number of nearby villages.

















Penkridge Train Station, which is located at the west of the village, is c. 900m to the south-west of The Site. It is accessible along the A449 to pedestrians / cyclists and via bus. The train station offers services to Stafford, Crewe and Liverpool to the north, and Wolverhampton and Birmingham New Street to the south. Therefore, The Site and settlement benefit from good connectivity to higher order settlements and the services and facilities they offer.

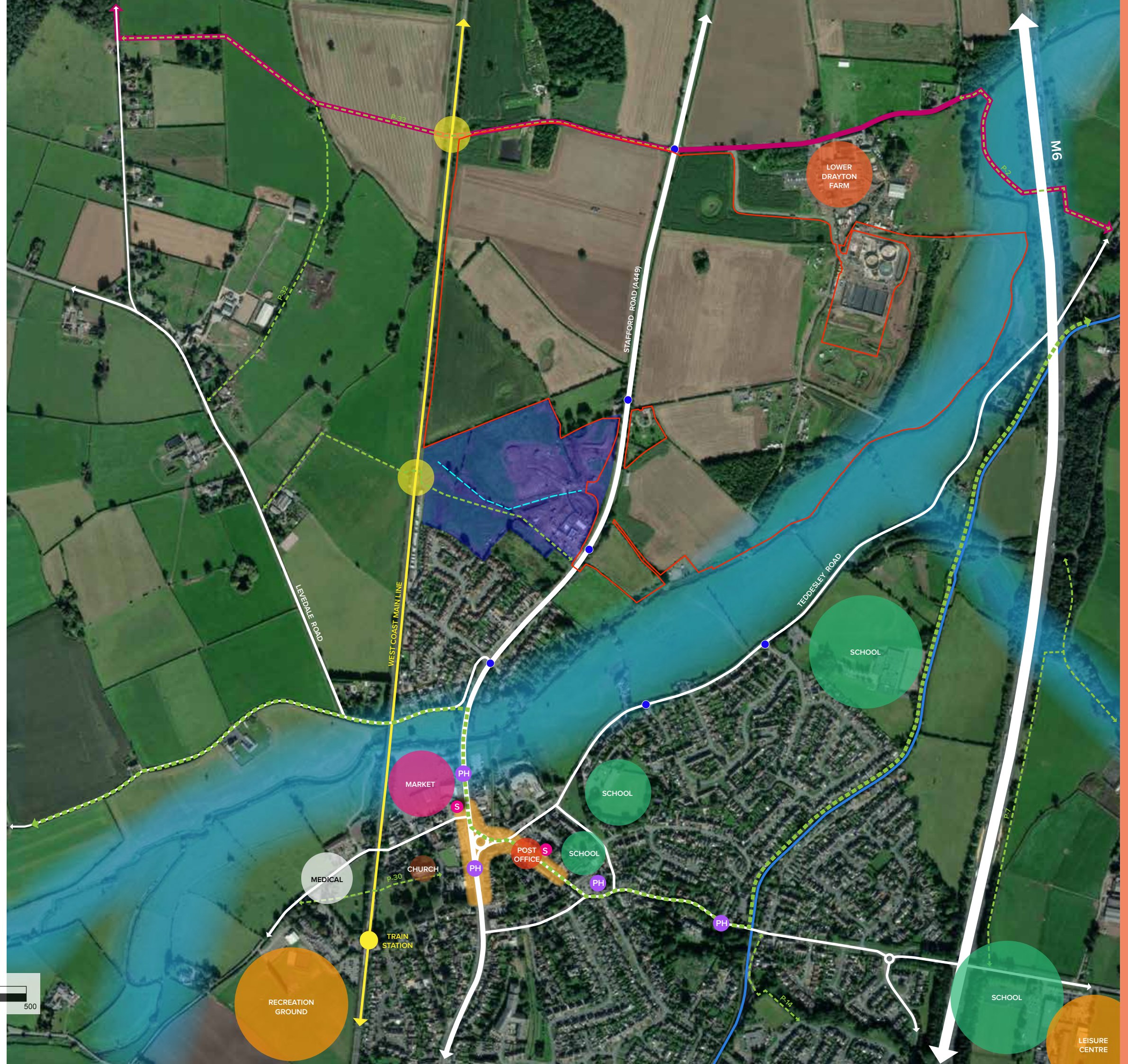
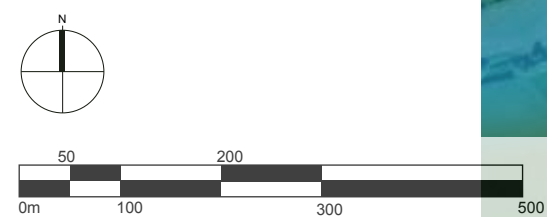
A strategic bridleway, Sabrina Way, runs to the north of The Site. It comprises some existing public rights of way and part of Lower Drayton Lane. From this, a footbridge provides access to countryside beyond the railway line to the west, and a footbridge and underpass provide access over the River Penk and beyond the M6 to the east.

Further south, a public footpath runs through the adjacent Bloor Homes development, and provides an elevated footbridge over the railway line.



## LEGEND

-  Site Boundary
-  Public Right of Way
-  Staffordshire Way
-  Sabrina Way (Strategic Bridleway)
-  Main road
-  Flood Zone
-  Railway Line
-  Local centre shops
-  Railway bridge (with pedestrian access)
-  Watercourse
-  Canal
-  Drainage channels (within site)
-  Housing development under construction
-  Public House
-  Supermarket
-  Bus Stop





## TOWNSCAPE CHARACTER

The Village Summary for Penkridge within the South Staffordshire Design Guide (2018) describes the village as;

*“dominated by its tightly enclosed landscape setting and the linear transport routes. These create strong, impermeable edges to the settlement’s boundaries, and a distinctively inward looking built form, arranged around the transport routes and separated from the enclosing, open countryside.*

*Many of the traditional buildings in and around Penkridge were constructed for workers on the Teddesley Estate. They are often characterised by substantial, well-detailed chimneys, fish scale banding of the roof tiles and decorative timberwork on gable ends. They give a specific local distinctiveness which is expressed more strongly in and around Penkridge than any other village in the District.”*

### Village Centre

The traditional centre of Penkridge is located to the immediate south of the River Penk and straddles the A449, with the church and railway station located to the west and the historic Market Place to the east. The central core is designated as a Conservation Area and contains a number of listed buildings. This includes the Grade I listed parish church, dating from the 13th Century and constructed of local stone, and its tower is a prominent landmark within the village. Elsewhere, the majority of buildings are generally around 2 storeys with an increase in height along Stafford Road.

### Settlement Growth

The village settlement pattern evolved from its historic function as a horse trading centre. The figure on Page 18 shows the broad growth of Penkridge over the 20th Century. Early expansion mainly constituted ribbon development along the main routes leading out of Penkridge, in particular to the south along Wolverhampton Road, to the east along Francis Green Road Lane, and to the north along Stafford Road. This was followed by further ribbon expansion and some infill housing estate development, most notably to the north east alongside the River Penk.

There was little development to the west, where the railway line forms an effective edge to the settlement.

The construction of the M6 motorway in the late 1960’s led to more rapid and extensive expansion of the settlement in the latter part of the 20th Century, mainly to the south and east. Housing estate development extended beyond the canal and commercial development was located along the southern fringes.

There has also been some modest infill development to the north of Penkridge along the A449 and extending west towards the railway line. This housing area is mainly typified by 2 storey detached houses and bungalows, with properties often featuring front driveways and integral garages. It has a very suburban character with small cul-de-sacs enclosed by repeated house types. Mainly red brick, some units have mock timber and render gables.

### A449 (Stafford Road) Corridor

The site straddles the A449 on the northern edge of Penkridge and forms part of the entrance into the village from the north. This stretch of road is currently defined by hedged enclosure on both sides and includes a row of distinctive mature trees



View of church tower across Stone Cross



Market Street



Stafford Road



along its eastern edge, close to the boundary with Old Drayton Farm, a cluster of traditional brick farm buildings including a barn conversion.

A new access roundabout and housing development, located to the immediate south of the western part of the site, announces the arrival into the settlement. This development, by Bloor Homes, is still currently under construction. It is mainly characterised by 2 storey red brick and rendered properties set within a framework of linear green corridors, which incorporate surface drainage features and retained vegetation. Features of the built development include the use of arched porches or lintels above doorways, painted render, casement windows and exposed timber effect gables. Some of these features can also be seen in the traditional vernacular within Penkridge.

Further south, more established residential development flanks the A449 as it continues into Penkridge. This is mainly characterised by larger detached and semi-detached housing set back from the main road within larger plots or accessed from service roads running parallel to the main carriageway behind tree lined grass verges. Double height bay windows, use of render, hipped roofs and mock-tudor style gables are also features of properties along this frontage.

### Outlying Development

There is a cluster of older buildings around Lower Drayton Farm to the north east of the site. This includes the farm house and brick outbuildings and a row of Grade II listed cottages that feature some remnant timber framing and distinctive gabled porches. Within the farm complex are also a number of large agricultural storage buildings, housing an indoor play centre and a maintenance depot. Further south, and immediately abutting the site, is an anaerobic digester plant set within a fenced compound. This includes a number of large domed structures that are visible from the surrounding area.



Bloor Homes development on approach from A449



Bloor Homes development to the south of the site



Cul-de-sac development to north of River Penk



Buildings at Old Drayton Farm



A449 Frontage



Grade II Listed Lower Drayton Cottages

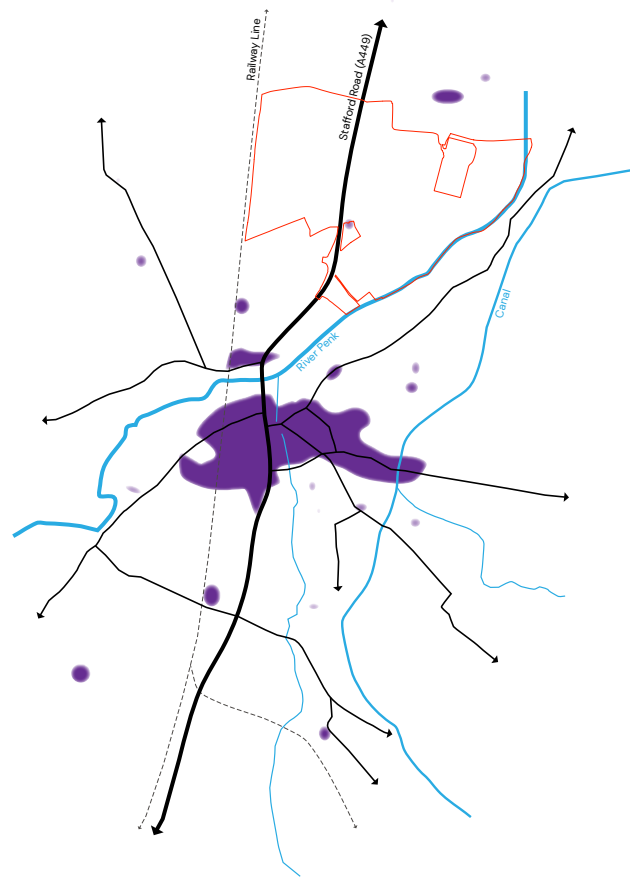


Buildings at Lower Drayton Farm

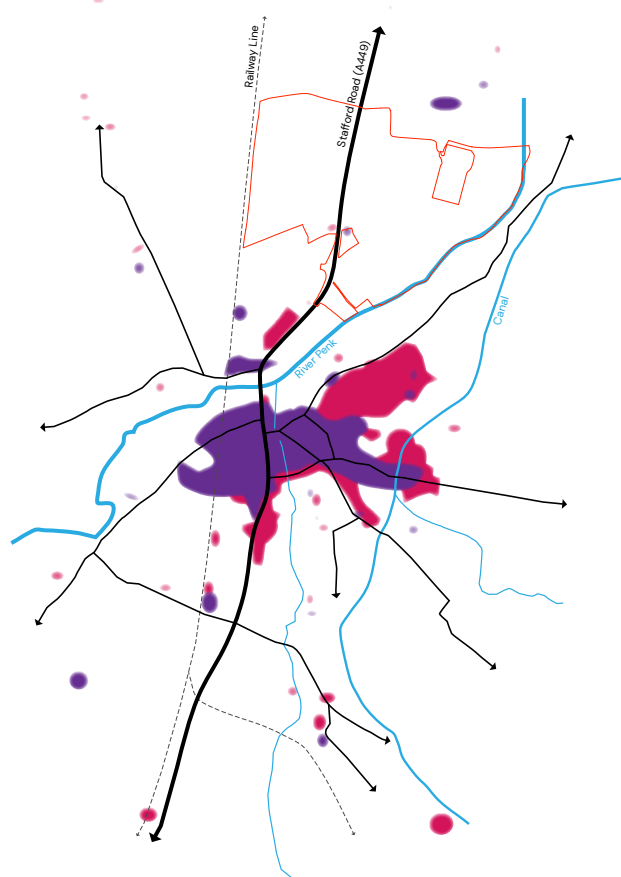


Anaerobic Digester Compound

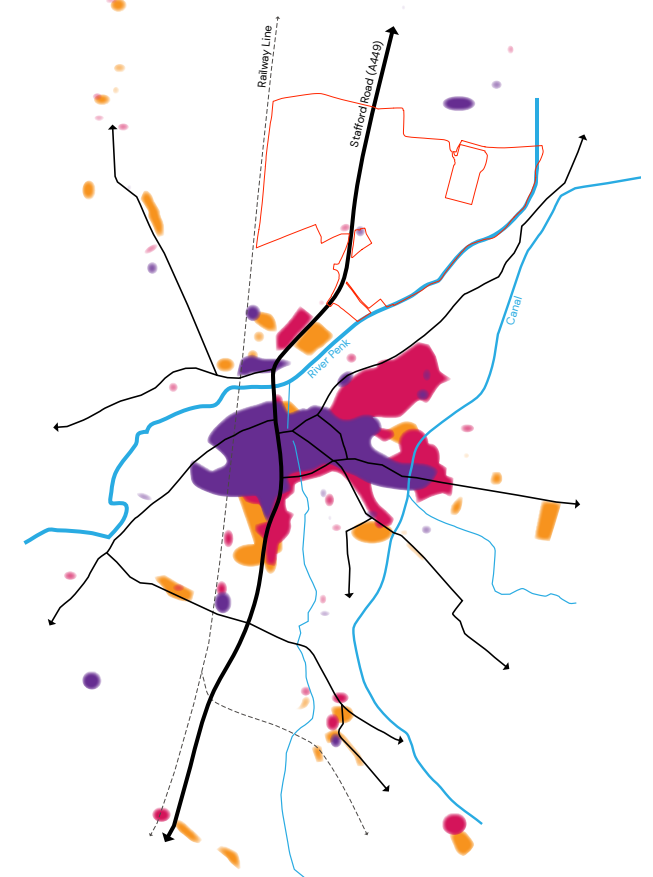




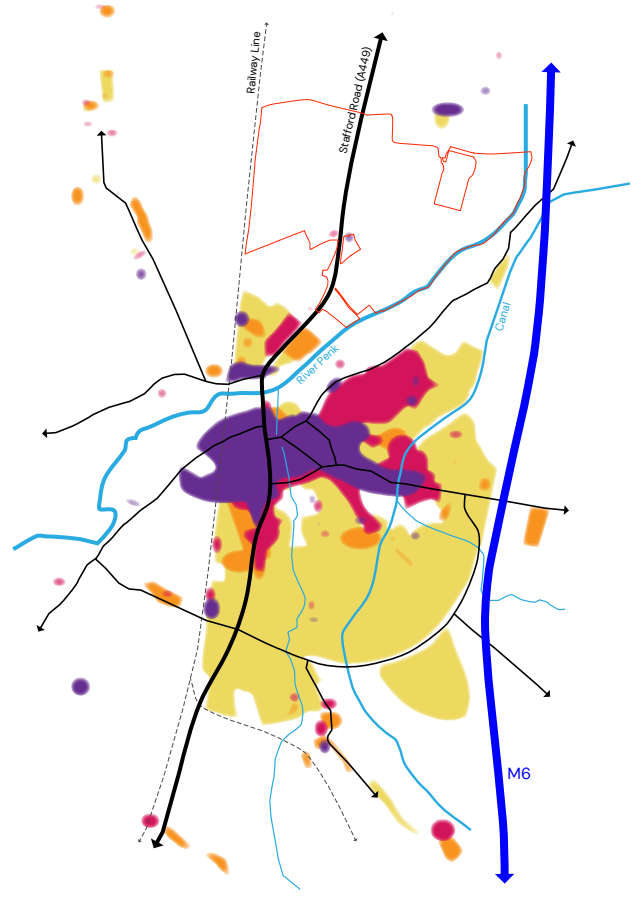
1924



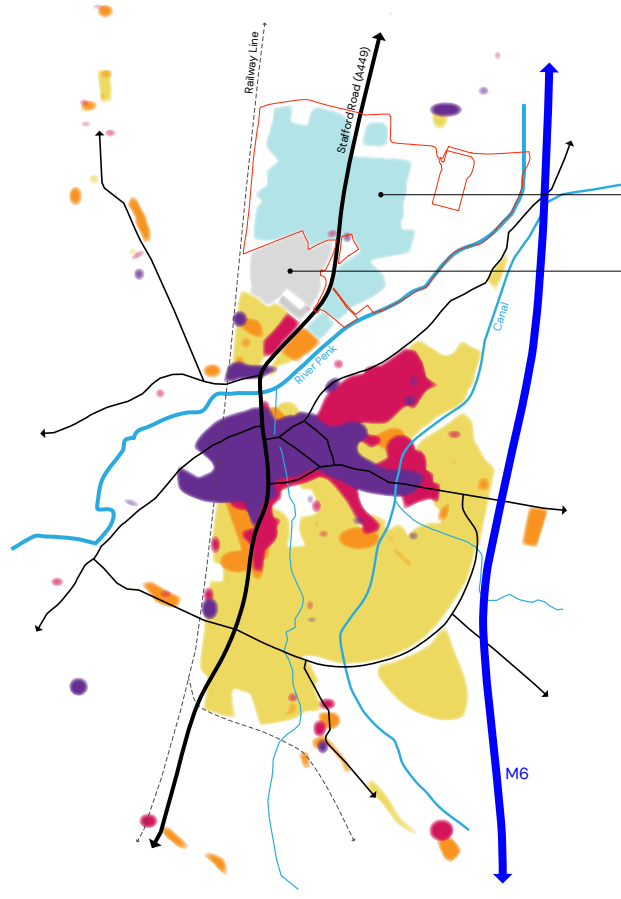
1954



1968



1994



PROPOSED

Proposed development  
Recent development



# Site Assessment

## THE SITE

The Site is located to the north of Penkridge both to the east and west of the A449 Stafford Road. It covers 69.5ha of largely agricultural land.

To the west of the A449, The Site's northern boundary is delineated by strategic bridleway CP33. To the east of the A449, the northern boundary is delineated by Lower Drayton Lane, the Lower Drayton Farm complex and an anaerobic digester (AD) plant.

The Site's eastern boundary is marked by the western bank of the River Penk, save for the foul pumping station that is not included within the site boundary. As such, the eastern fringes of The Site form part of the functional flood plain.

The Site's western boundary is delineated by the West Coast Mainline, which is in-cut where it travels past The Site. The West Coast Mainline travels north-south along the western edge of Penkridge and marks the westernmost extent of the built development.

The Site is largely comprised of agricultural land, with smaller pastoral fields to its south and larger arable fields to the north.

The Lower Drayton Farm complex is located immediately to the north east of The Site, and itself incorporates a number of buildings including a play barn, repair workshops, a B&B and an anaerobic digester (AD) plant. Some related uses are located in The Site, which includes pumpkin picking and a motocross area (to the south of the AD plant).

## TOPOGRAPHY & SITE FEATURES

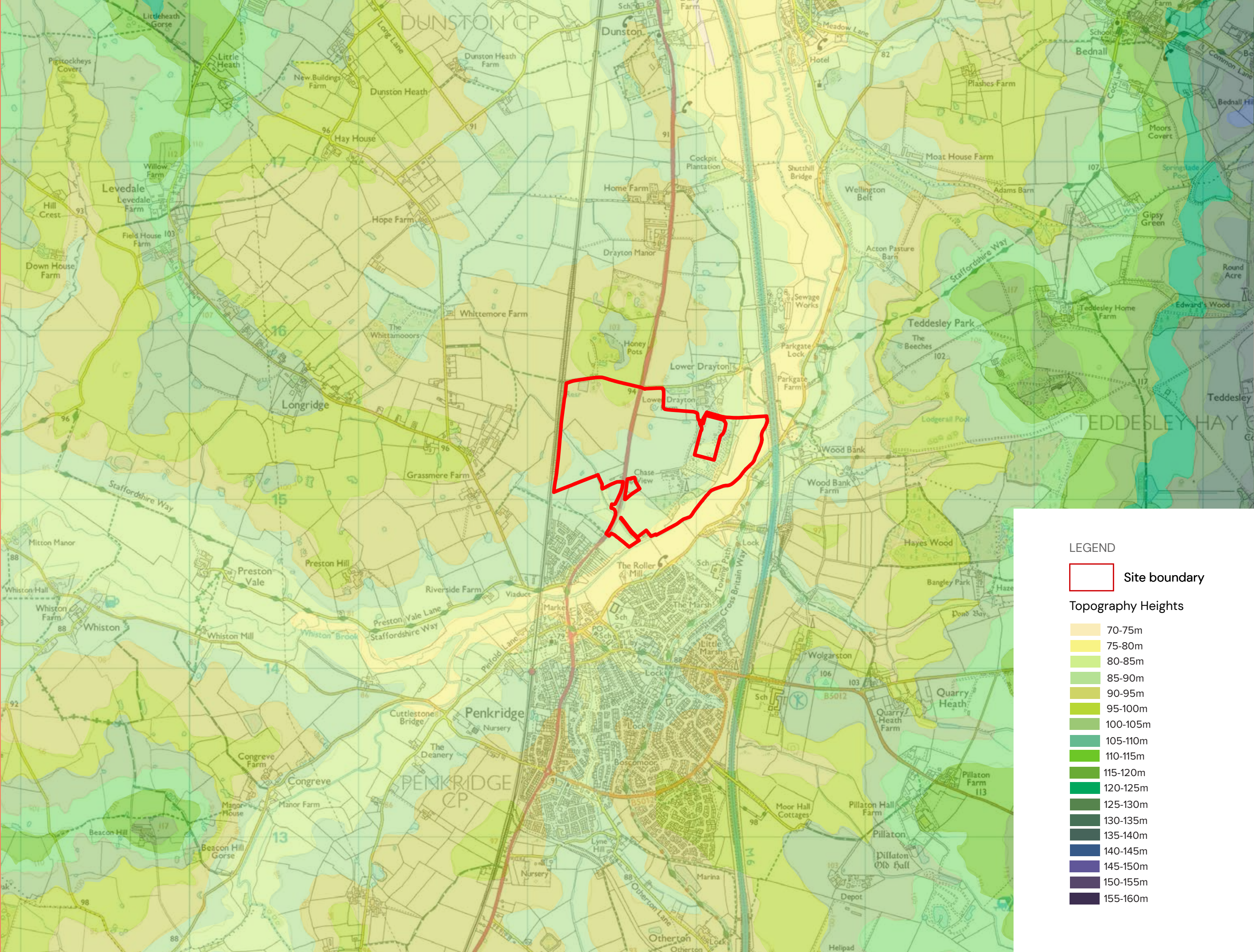
The site gently slopes from a high point of c. 96.5m AOD on the northern boundary to c. 76m to 77.5m AOD on the eastern boundary.

Hedgerows and tree belts provide a strong sense of enclosure along many of the field boundaries, particularly on lower ground to the south. Existing woodland provides further enclosure along the eastern fringes of the site, helping to define the rising ground alongside the River Penk corridor, and is prominent on higher ground to the north.


The Site drains to the south and east in the direction of the River Penk, with existing drainage ditches forming part of the green corridor network within the adjacent development currently under construction to the south.

There are a number of field ponds scattered across The Site and within the existing woodland and some storage lagoons within the north western corner.























LEGEND

 Site boundary




Topography Heights

	70-75m
	75-80m
	80-85m
	85-90m
	90-95m
	95-100m
	100-105m
	105-110m
	110-115m
	115-120m
	120-125m
	125-130m
	130-135m
	135-140m
	140-145m
	145-150m
	150-155m
	155-160m





LEGEND

-  Site boundary
-  Waterbodies
-  Woodland



LANDSCAPE DESIGNATIONS

The Site is not subject to any statutory landscape designations, and is not identified as a valued or protected landscape as defined by NPPF paragraph 174.

Cannock Chase AONB is located c. 3.2km to the east of The Site. It is designated to protect the associated landscape of heathland and wood pasture. The Management Plan identifies that *“the landscape of the Chase is varied and there are different elements that are distinctive in different parts of the AONB, including the inspiring views and open prospect to and from parts of the high heathland plateau; the secluded and ‘secret’ valleys and tracks in the forested areas; and the patterns of small fields, farmsteads and parkland along the northern and eastern edge of the Chase”*.

The nearest public access land within the Cannock Chase AONB is located c. 4.7km to the east of The Site.

The Site is also located adjacent to, but outside of, the designated Green Belt; which is located to the south-east of the River Penk. Therefore, it is not subject to the policy implications and constraints that are associated with that designation.

The Site is not subject to any local designations that are relevant to the overall landscape value.

LANDSCAPE CHARACTER

The Site is located in the south-eastern corner of NCA ‘61: Shropshire, Cheshire and Staffordshire Plain’ which is an expanse of flat or gently undulating, lush, pastoral farmland, with a strong field pattern and generally well-maintained hedgerow boundaries. Land to the east of the M6 is located in the NCA 67 ‘Cannock Chase and Cank Wood.’

The “Planning for Landscape Change” SPG identifies The Site in the easternmost fringes of the ‘Ancient Clay Farmlands’ LCT. The Site and its immediate surroundings display many of the characteristics of that LCT. It lies within a rolling lowland plain, with a mix of farmland types, with smaller, enclosed fields adjacent to the settlement edge giving way to larger arable fields to the north. Urbanising impacts are provided by the housing edge, the railway line, the nearby M6 and A449 and the AD plant. The church tower in Penkridge is a local landmark in the surrounding landscape.

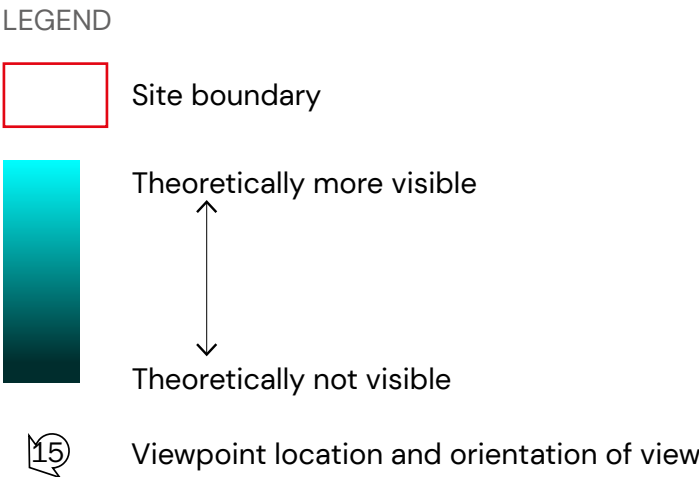
VISUAL AMENITY

Analysis of the visual conditions associated with the development has been informed by bare earth Zone of Theoretical Visibility (ZTV) mapping, which identifies the locations from which development is theoretically visible from. Given that the ZTV does not take account of screening offered by the built form or existing vegetation, further field assessment has been undertaken to appreciate the extent to which views are limited or available.

The ZTV mapping and fieldwork determines that clear open views of The Site are only available from within The Site and from locations close to the Site to the west, east and north. Often beyond a distance of 400m, the Site is not visible due to a combination of changing topography, existing urban features (roads and the railway in particular) and the intervening vegetation within the relatively flat landscape.

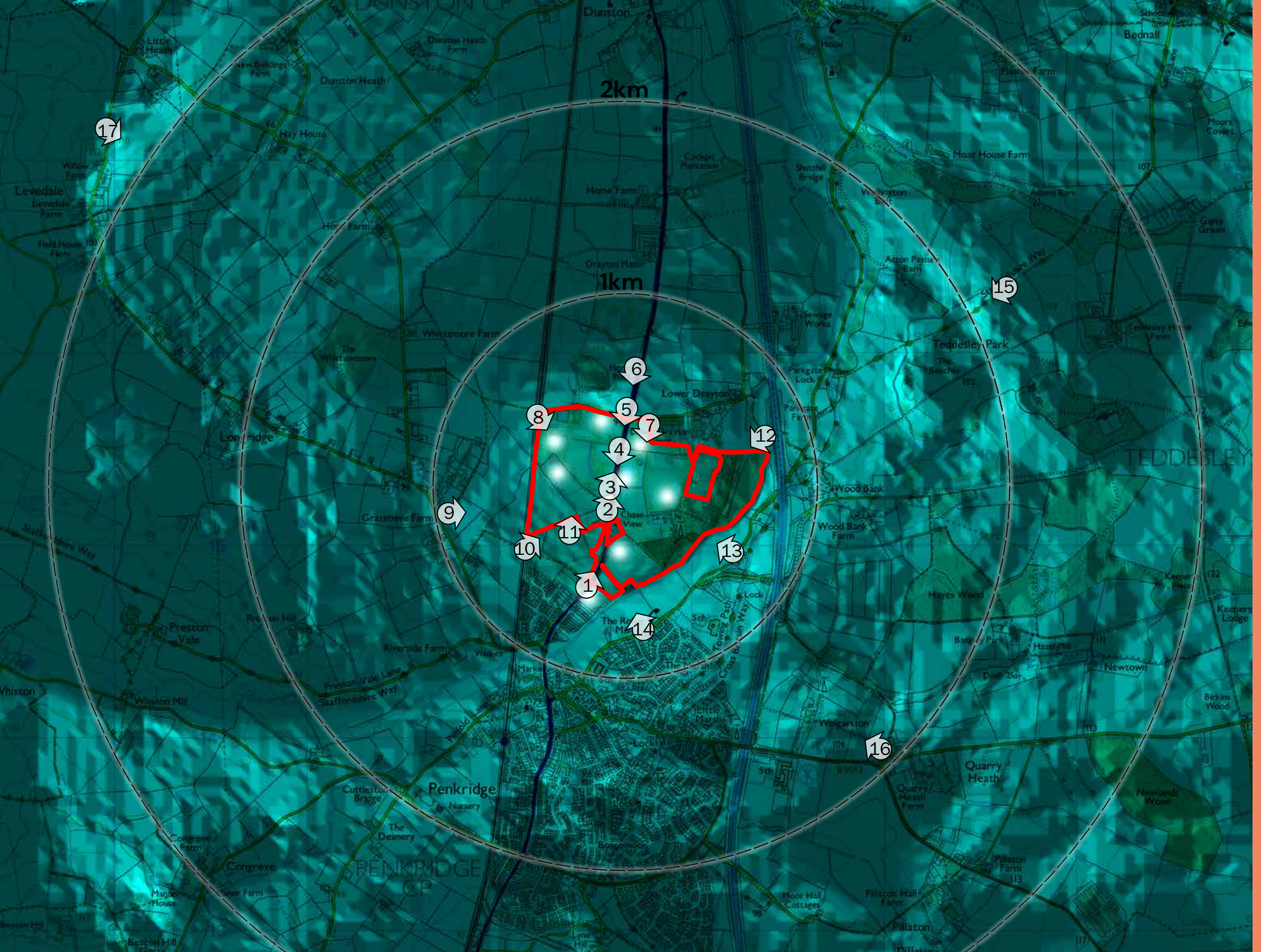
It is only from rising ground further to the east (beyond 1km) where views of The Site become available and increasingly so as the land rises further in the vicinity of Cannock Chase (over 3km away). However, there is a lack of potential public receptor locations (footpaths / bridleways / roads), in that area, and woodland around Cannock Chase also restricts the ability to view over the wider landscape towards The Site.

The following pages present a selection of the key views that have been agreed with SCC’s Landscape Officer.



Note: The ZTV is based on a ‘worst-case’ terrain only scenario, and does not include potential screening from hedges, trees, woodland, buildings, structures or built-up areas.







## KEY VIEWS



**View 6:** Stafford Road, north of The Site.



**View 9:** Public footpath Pe32, west of The Site.



**View 12:** Public bridleway Pe2 (Sabrina Way), north-east of The Site.





**View 13:** Teddesley Road, east of The Site.



**View 14:** Teddesley Road, south-east of The Site.



**View 15:** Public bridleway TH1, Staffordshire Way, north-east of The Site.



## DRAINAGE

The River Penk is located adjacent to the eastern boundary of The Site. The east of The Site, which is significantly lower lying relative to the remainder of The Site, forms part of the River Penk's floodplain. A single watercourse runs north-west to south-east through The Site to the west of the A449, along a field boundary. There are also a small number of ponds located within The Site.

The Site is largely located within Flood Zone 1, aside from the lower-lying areas associated with the River Penk and its floodplain; which are located in Flood Zones 2 and 3. The risk of surface water flooding largely correlates with that, with the vast majority of The Site at very low risk of surface water flooding. There are some sporadic areas of higher surface water flood risk which (in addition to the Penk's floodplain) relate to the watercourses within The Site and localised topographical depressions.

## ECOLOGY

The Site and its immediate surrounds are not subject to any statutory ecological designations, and there are no non-statutory designated sites within or in immediate proximity to The Site.

The Cannock Chase Special Area of Conservation (SAC) and Site of Special Scientific Interest (SSSI) are located c. 4.6km to the east of The Site. Motte Meadows SAC and SSSI are located c. 7.8km to the south-west of The Site. The Pasturefields Salt Marsh SAC and SSSI are located c. 10.9km to the north-east of The Site.

An Ecological Appraisal has been undertaken for The Site. It finds that the majority of the western and central parts of The Site are arable, with rough grassland at the margins of the arable fields. A single field to the west of the A449 and the areas towards the east of The Site comprise species-poor semi-improved grassland. There is a rectangular area of improved grassland to the west of the AD plant. There are also areas of broad-leaved woodland, and sporadic areas of bare ground, rough grassland and scrub.

A number of species surveys have been undertaken. A badger sett has been recorded within The Site and it is considered that the arable land, rough grassland margins, scrub, semi-improved grassland, trees and hedgerow offer some suitable foraging opportunities. six trees have been identified as having potential to support roosting bats and some bat activity has been recorded within The Site.

Notable bird species were identified in the Phase 1 Habitat Survey, including species that are on the Red Listed and Priority Bird Species List, and Schedule 1 bird species. Some of the habitats within The Site offer suitable foraging and nesting opportunities for some bird species.

No evidence of Water Vole or Otter was recorded along the River Penk, but it is likely that both species utilise the river. Some ponds within and adjacent to The Site have a small Great Crested Newt population.

No mammals were recorded within The Site, but it is likely that a variety of common mammal species would use the hedgerows, woodlands, semi-improved grasslands and arable habitats.

## HERITAGE

There are no designated or non-designated heritage assets within The Site, nor is The Site located within a Conservation Area (CA).

There are, however, some listed buildings within the vicinity of The Site. To the north are the Grade II listed Lower Drayton Cottages and Lower Drayton Bridge. A Grade II listed railway viaduct is located c. 550m to the south-west of The Site. The remainder of the listed buildings in proximity of The Site are associated with the two nearby CAs.

The Staffordshire and Worcestershire Canal CA is located to the north and east of The Site. The nearest listed building within it is the Grade II listed Staffordshire and Worcestershire Canal Bridge that is located c. 260m to the east of The Site.

The Penkridge CA is located to the south. The nearest listed buildings are located at Teddesley Road; the Grade II listed 'The Cottage' (c. 200m from The Site) and the Grade II listed Mill End (c. 300m from The Site). The majority of the other listed buildings within the CA are located around the village centre.

The Old Drayton Farm building that is located to the east of the Stafford Road has some local heritage interest. It is considered that this is commensurate to a non-designated asset with low importance.

The potential for archaeological remains within The Site is considered to be low, as confirmed by SSC's 2022 Historic Environment Site Assessment (HESA).



## UTILITIES

A single National Grid Electricity Distribution 11kV overhead line enters The Site at its south-eastern corner, and runs broadly northwards before exiting The Site near to the Lower Drayton Farm complex.

A 250mm medium pressure gas main runs through the centre of The Site along the A449. The main turns to the east near to the north of The Site, before exiting The Site and continuing to the Cadent Gas Yard that is located to the north-east of The Site. An easement is associated with the gas main where it travels through the internal area of The Site.

A 225mm and 4" clean water main (South Staffs Water) is present within the carriageway of the A449. Two 225mm foul water sewers (Severn Trent Water) enter The Site at its south. The first of which heads in a north easterly direction towards the pumping station adjacent to The Site's south-eastern boundary. The second enters the site from the south of Stafford Road, heads into the sewerage pumping station and, from there, a 450mm rising sewer heads in a northerly direction towards the northern boundary of The Site.

## ODOUR

A 140m offset has been incorporated within the Strategic Masterplan between built development and the AD plant that is located to the north-east of The Site.

An odour assessment has been undertaken to consider the potential impact of the AD plant on amenity, taking account of the proposed layout. It establishes that odour associated with the AD plant can be detected with easterly winds, but not at a level or frequency considered to significantly influence amenity under the prevailing south-westerly winds.

Notwithstanding that, and whilst easterly winds are not prevailing, mitigation measures will be put in place to ensure that the influence of odour on residents' amenity is minimised in such circumstances. That includes the provision of intervening planting between the AD plant and dwellings; which will be secured through the detailed design process.

The odour assessment confirms that, subject to the delivery of such mitigation, it is unlikely that odours would be experienced at highly sensitive receptor locations at a frequency or intensity that would influence amenity.

## NOISE

The dominant sources of noise associated with The Site relate to the road traffic on the A449 and M6, the existing railway line at The Site's western boundary and the day-time operation of the AD plant.

High-level acoustic assessments have highlighted that appropriate mitigation can be accommodated by incorporating required development offsets and / or through the detailed design of buildings.

## AGRICULTURAL LAND AND SOILS

An Agricultural Land Assessment has been carried out in relation to The Site.

It found that 36% of the land is of Grade 2 standard, which is largely located to the immediate west of the A449 and around Lower Drayton Farm and Old Drayton Farm to the east of the A449, with a further small area at the north-western corner of The Site. 12% of The Site is Sub Grade 3a, which largely relates to the margins of the Grade 2 land. The remainder of the land under agricultural use comprises Sub Grade 3b (28% of The Site). 24% of The Site comprises non-agricultural land, including farm tracks, waterbodies and wooded areas.

## MINERALS

The Site falls within a Mineral Safeguarding Area for sand and gravel.

However, the Mineral Resource Assessment (MRA) concludes there is no potential value in extracting the minerals as it would not be commercially viable, that there is no prospect of prior extraction, that there is an overriding need for the proposed development, and that the introduction of the residential receptors will not result in proximal sterilisation of the wider MSA.





























# Opportunities and Constraints

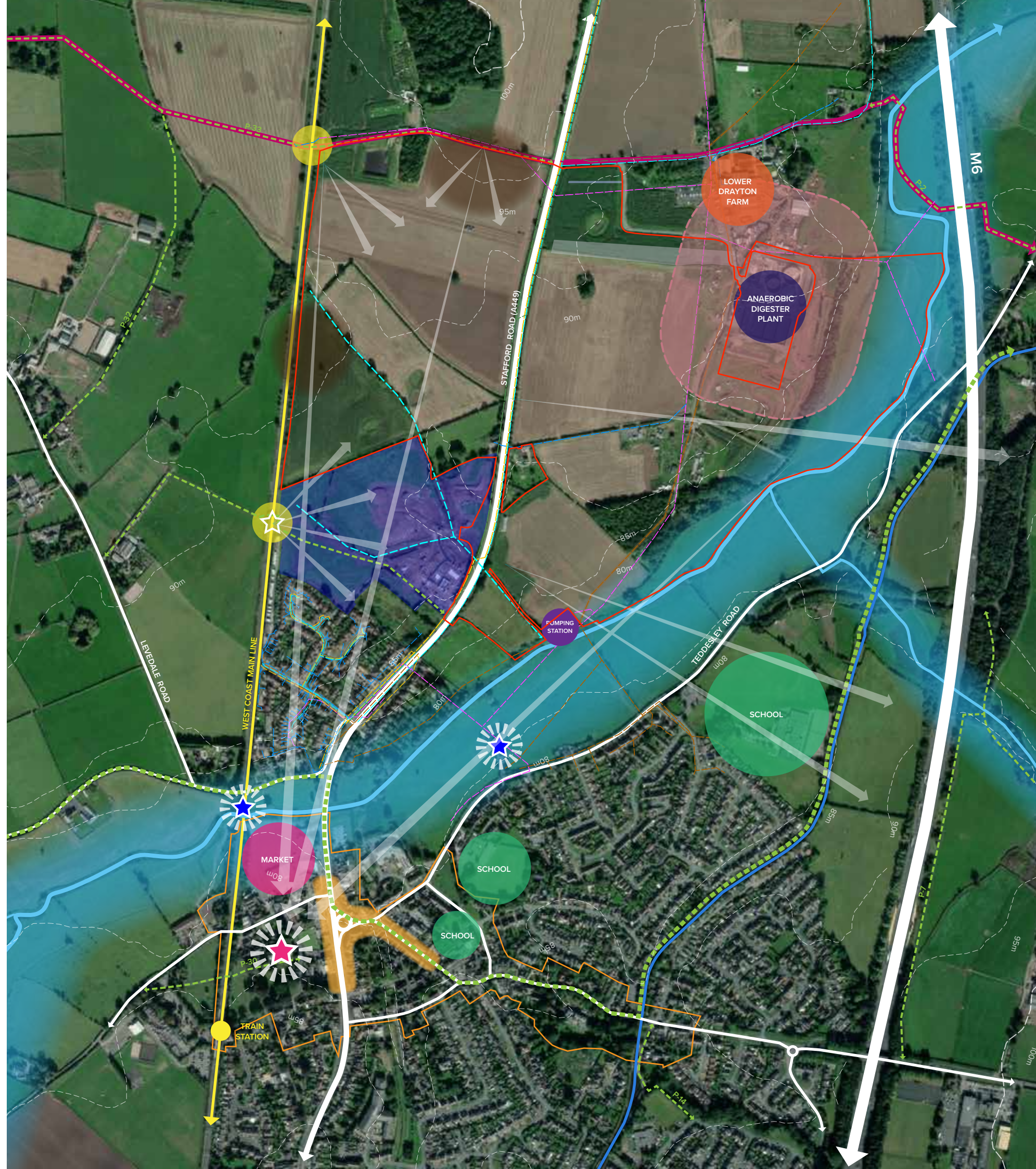
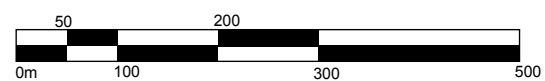
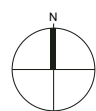
The key features in relation to The Site and its surroundings are as follows:

- The Site is located to the north of Penkridge and forms a logical extension to it. It is in close proximity to the existing village centre and benefits from direct connections to the village centre via bus, walking and cycling, and therefore benefits from the services and facilities that are on offer therein.
- The Site broadly rises from south to north along the A449, and peaks to the ridgeline that runs broadly north – south through The Site to the west of the A449;
- The existing drainage pattern directs water towards the east of The Site, discharging into the River Penk / watercourses within The Site;
- The eastern fringes of The Site comprise the functional floodplain of the River Penk;
- The Site is not of any particular value in landscape character, and does not share the characteristics of the more valued land to the east (i.e. the Cannock Chase AONB);
- The Site is relatively well-contained visually and physically by the existing built form and vegetation in the surrounding area. The visual envelope is, therefore, largely contained to the areas in proximity to The Site;
- There is some visibility between The Site and the higher land to the east (i.e. rising towards the Cannock Chase AONB). The Site would, however, be viewed at a significant distance and would be seen as sitting on the opposite side of the valley in the context of the existing built form.
- The tower of the Grade I listed Church of St Michael and All Angels is a landmark building in the settlement. There is a key view corridor between the visually exposed higher ground at the northern boundary of the site and the church tower;
- Other listed buildings / structures in proximity to The Site are located to its north-east near to the Lower Drayton Farm compound, at its south-west at the river viaduct, and within the two CAs that are in proximity to The Site;
- The existing vegetation creates a varied character across The Site. The area to the south comprises smaller, irregularly shaped parcels that are well-enclosed, whereas the parcels at the north are larger and much more visually open.
- The A449 runs through the centre of The Site, providing access between Penkridge village centre and Stafford, Wolverhampton, Kidderminster and Worcester;
- The West Coast Mainline runs along the western boundary of The Site, providing services from Penkridge Train Station to the south-west of The Site;
- A public bridleway (Sabrina Way) runs to the north of The Site, comprising existing public footpaths. The bridleway provides vehicular access to the Lower Drayton Farm compound and the AD plant that is located to the north of The Site.
- A public right of way is located near to The Site's southern boundary, and provides access between the A449, the adjacent Bloor Homes development, and the open countryside beyond the trainline to the west of The Site;
- An AD plant is located adjacent to the north-eastern boundary and needs to be accounted for in terms of odour and noise;
- A foul pumping station is located adjacent to The Site's boundary at its south-east.



## LEGEND

-  Site Boundary
-  Public Right of Way (PROW)
-  Staffordshire Way
-  Sabrina Way (Strategic Bridleway)
-  Main road
-  Contour
-  Landmark Church Tower
-  Landmark Structures/Buildings
-  Pedestrian Footbridge overlooking the site
-  Visually exposed higher ground
-  Key Views
-  Flood Zone
-  Railway Line
-  Anaerobic digester plant buffer
-  Local centre shops
-  Railway bridge with pedestrian access
-  Penkridge Conservation Area
-  Watercourse
-  Canal
-  Drainage channels within site
-  Housing development under construction
-  Area safeguarded for gas connection
-  Existing HV electricity apparatus
-  Existing LV electricity apparatus
-  BT apparatus
-  Foul water drainage (underground) with 3m easement
-  Mains water
-  Gas pipeline (medium pressure)







4

# THE VISION

# The Vision

## EMERGING LOCAL PLAN VISION

The eLP provides a “Vision” for the development of the site and a range of key objectives that together with Policy SA4 seek to ensure the creation of a sustainable new neighbourhood as illustrated in an Indicative Concept Plan.

*“The Land North of Penkridge will provide a new northern edge to the settlement and be a sustainable extension and new neighbourhood to the village, delivering high quality, distinctive development integrated within a strong landscape setting, including a new Community Park area, providing a range of formal and informal open space and uses, respecting and enhancing the area’s natural assets and key views.*

*The development will have a strong environmental, economic and social relationship with the existing settlement, complimenting the existing village centre and facilities, building on its existing/future education opportunities, and delivering new centrally located space for community, retail and local business/employment, including a new first school and a local convenience store.*

*The new homes and facilities will increase the range of opportunities for existing and future residents, including bungalows/single storey homes.*

*Land north of Penkridge will be well-connected to the existing settlement and surrounding countryside with excellent public transport, cycling and walking links. The connections will enhance the development’s integration with the existing settlement, encouraging active travel to services and facilities, including the rail station, education and the new Community Park.*

*High quality, distinctive design, set within a green and blue infrastructure framework, extending throughout and beyond the new development, will provide a new framework of greenways and deliver a strong sense of place identity. The development will focus upon achieving high environmental standards, energy efficiency and achieving a regulated zero carbon community.”*

## OUR SHARED ASPIRATION FOR THE NEW NEIGHBOURHOOD

Bloor Homes and St Phillips share the Council’s aspiration for the creation of a vibrant new neighbourhood for Penkridge that provides much needed new sustainable homes along with essential community services that will meet the day to day needs of its residents.

It will be a sustainable neighbourhood that is closely connected to the existing village with a true sense of place, shaped by a green and blue infrastructure framework that reflects the local landscape character, ensures the health and well-being of its residents and maintains a connection with nature.

The eLP’s allocation policy requirement and vision and objectives for the development of the site have informed the preparation of the following aims and strategic principles that will underpin the Masterplan for the new neighbourhood and its complementary community, access and movement and green infrastructure strategies.

# Community Strategy

## Aim:

*An attractive and vibrant neighbourhood will be created, with a range of homes for all needs, local facilities, sustainable transport opportunities and open green spaces. This will provide the foundations for a healthy and happy new community that is sensitively integrated into its environment.*

## Strategic Principles:

A compact and well-connected community will be created, which will deliver high-quality, new housing in a range of sizes, types and tenures that will be popular, sustainably designed, and built to last to make a significant contribution to meeting the housing needs of the District;

- The development will include a community hub that is centrally located and is well-integrated with key movement routes through the Site and to Penkridge village centre;

- The Community Hub will incorporate convenience retail uses that offer an alternative to the village centre uses, but does not compete with it;
- A site will be provided for the delivery of a first school and nursery in an accessible location in the heart of the new neighbourhood;
- The co-location of the local centre and school will ensure that the uses are mutually supportive;
- The layout, form and character of the residential areas should respond to the site's context to provide distinctive places that are responsive to their localised setting and the public realm;
- The residential development should be set out in perimeter blocks to positively address the streets

and areas of open space, whilst also securing private spaces to the rear;

- Specialist older people's housing will be provided to meet housing needs in that regard;
- A range of POS will be provided throughout the neighbourhood to meet the needs of residents, providing a sense of community and ensuring social inclusion. As well as addressing the requirements for a riverside community park, that should include sports, recreation and play facilities, opportunities to grow food and informal spaces for social activity.





# Access and Movement Strategy

## Aim:

*Ensure that the development functions effectively as an integrated new neighbourhood of Penkridge with an emphasis on sustainable travel options to help minimise use of the private car. The network of greenways, cycleways and footpaths will facilitate and encourage active travel, contributing to residents' health and well-being.*

## Strategic Principles:

- Provide a strategic active travel route through the development and extend and enhance pedestrian and cycle infrastructure along the A449 into the village centre;
- Create an extensive network of attractive and safe active travel routes through the new neighbourhood on desire lines between the residential areas and key community facilities;
- Enhance the A449 corridor as it passes through the site to ensure that it does not sever the new neighbourhood, reduces traffic speeds, and

includes safe active travel connections across it. This should be reinforced through an appropriate design response within the built form;

- The form of vehicular junctions on the A449 into the development should provide safe access into the development areas and maintain the strategic function of the road;
- Integrate the existing public transport facilities and services with the development;
- Within the site, housing should be accessed via a clear hierarchy of connected streets.





# Green Infrastructure Strategy

## Aim:

*An expansive green & blue infrastructure network will ensure that the natural environment is part of the community's everyday life, providing opportunities to meet and play, strengthening social connections and encouraging active travel, as well as providing landscape and ecological enhancements.*

## Strategic Principles:

- Retain, enhance and reinstate the site's natural assets, including hedgerows, trees, woodlands, ponds and drainage corridors;
- Create an extensive network of connected green corridors and spaces to structure the built form, provide a green setting for the dwellings, provide access the natural environment, encourage active travel and to provide opportunities to meet and play;
- Incorporate new tree and woodland planting at the edges of the development to help assimilate the development into the existing settlement edge, soften views from the surrounding area and where possible frame / maintain views towards the church tower;
- Respond to the underlying topography and existing drainage patterns by retaining and enhancing the existing ditches and watercourses, and incorporate new SUDS features at localised low points to help positively manage surface water run-off;
- Deliver a minimum of a 10% biodiversity net gain across the site.





5

# THE MASTERPLAN

# Concept

*The aspiration for the development of the site is to create an attractive and sustainable environment that responds to the site's setting; retaining natural key features, and taking advantage of the existing landscape framework to create a high quality and distinctive development with a sense of place that is well integrated into its surroundings and closely connected to the village.*

*A concept has been developed to respond to the matters outlined in the preceding section and set out a broad framework for the masterplan.*

The overarching principles that underpin the design concept are illustrated on the adjacent page and are as follows:

- 1 Perimeter open spaces along the northern and western edges of the site will provide a landscape buffer between the development, railway line and strategic bridleway route and will maintain view corridors south towards the church tower in Penkrige. This will be framed by new native planting to strengthen the vegetated character along these edges and tie in with the existing woodland blocks.
- 2 Retained existing hedgerows will form the basis for a framework of green corridors within the site, which will also incorporate active travel routes, SuDS, habitat creation and uses to support the surrounding community. In the north western part of the site, where this existing structure is lacking, new hedgerow features and green corridors will be created, based on the alignment of historic field boundaries. The 'Green Spines' will form the primary green links within the site, connecting the central community hub with strategic public rights of way and the new community parkland.

- 3 Internal roads will provide vehicular access into the new development, with a clear street hierarchy distributing traffic around the site and diverting vehicles away from the A449.
- 4 The existing drainage pattern will provide the basis for a proposed blue corridor network across the site, working with the underlying topography and incorporating a number of sustainable drainage features (swales, basins) to manage the transfer of water across the site.
- 5 Promote connections between the proposed development and the adjacent new housing scheme to the south west, including a road connection into the proposed development off the existing roundabout on the A449.
- 6 Incorporate key view corridors into the proposed development – maintaining views towards the church tower that is a focal point for the wider existing community.
- 7 Promote active travel and public transport connections between the new development and the existing village centre along the A449 corridor, encouraging residents to access existing services and facilities within Penkrige without using the car, including the train station.
- 8 Provide compact new housing areas within easy reach of the community hub that positively address the surrounding open space network, whilst forming a natural expansion of the existing settlement northwards.
- 9 The community hub will contain a local level of services and form an important focal point for the new community, located centrally within

the development along the A449. It will have a distinctive identity inspired by local character but will remain subsidiary to the existing village centre.

- 10 The proposed development will positively re-define the northern threshold of the village, with distinctive built form and a new roundabout access providing a gateway feature on the approach into Penkrige along the A449.
- 11 A number of existing landscape features, including prominent mature trees, will be incorporated into the green corridor network and retained as focal points within the development.
- 12 An open space buffer surrounding the AD plant will incorporate sports pitches, SuDS, habitat creation, native planting and amenity/ community green space.
- 13 The existing woodland blocks will be retained and strengthened with additional tree planting to maintain a strong wooded edge to the top of the slope running alongside the River Penk. Strategic breaks will allow views out towards Cannock Chase in the east.
- 14 A riverside community park will be located within the flood zone alongside the River Penk. It will have a more naturalistic treatment, providing opportunities for informal recreation and dog walking. Tree planting along the north eastern edges will help to soften the impact of the elevated M6 while maintaining the linear character of the riverside corridor and framing views back towards the church tower in the centre of Penkrige.







# The Masterplan

The Masterplan for the development has been prepared with a clear knowledge and understanding of the specific characteristics of the site and its surroundings, and draws on best practice in relation to place making. It reflects the shared vision and concept for the new neighbourhood and has evolved to respond to the outcomes of the stakeholder and public engagement that has been undertaken.

The Masterplan seeks to optimise the scheme's capacity to accommodate the development requirements established in Policy SA1 and ensure its co-ordinated delivery. It demonstrates the site's capacity to deliver up to 1,100 new homes (including a minimum of 40 units of specialist older people's housing), a community hub incorporating retail uses, other commercial premises and / or work space, hot food takeaway / drinking establishment, a mobility hub and (if required) community space, a new first school and nursery site, and extensive public open space including sports and play facilities.

The core aim of the Masterplan is, therefore, to ensure the delivery of a vibrant new neighbourhood for Penkridge that will provide much-needed homes and community facilities to address the housing needs of the Borough. It will deliver a sustainable neighbourhood that is closely connected to the existing village, with a sense of place that is shaped by its green and blue infrastructure framework. This will create a place where people can live, work and play in ways that support health and wellbeing, while mitigating climate change and contributing to the success and vitality of a new community.

The Masterplan is a compilation of the above Community, Access & Movement, Green Infrastructure and Delivery Strategies that themselves reflect the requirements of Policy MA1 in the eLP. These strategies and the key development parameters and principles that underpin them are set out in the following sections.

## LAND BUDGET

### SITE AREA

69.51  
HECTARES

### CAPACITY

UP TO 1,100 UNITS

(INCLUDING AFFORDABLE & SPECIALIST  
OLDER PEOPLE'S HOUSING)

### A COMMUNITY HUB

INCLUDING RETAIL, COMMERCIAL PREMISES  
AND / OR WORK SPACE, HOT FOOD  
TAKEAWAY / DRINKING ESTABLISHMENT,  
COMMUNITY SPACE (IF REQUIRED /  
DESIRABLE) AND A MOBILITY HUB

A NEW FIRST  
SCHOOL &  
NURSERY SITE:  
1.5 HA

### A RIVERSIDE COMMUNITY PARK

c. 7.3<sub>HA</sub>

### GREEN & BLUE INFRASTRUCTURE: C. 27 HA

INCLUDING PUBLIC OPEN SPACE, SUDS,  
EXISTING VEGETATION AND NEW TREE /  
HEDGE PLANTING

FORMAL SPORTS PITCHES  
C. 2.94HA (AND ASSOCIATED FACILITIES)







# Delivery Strategy

The implementation of the Masterplan will ensure the comprehensive and co-ordinated development of the allocation site.

## Application Strategy

Three co-ordinated planning applications for the respective land under the control of Bloor Homes and St Philips have been submitted to South Staffordshire Council for its consideration and approval:

- Outline planning application (OPA) in relation to land controlled by St Philips in the northern part of the allocation site to both the east and west of Stafford Road for the delivery of up to 750 dwellings and supporting infrastructure;
- OPA in relation to land controlled by Bloor Homes in the southern part of the allocation site to the west of Stafford Road for the delivery of up to 215 dwellings and supporting infrastructure;
- Full planning application (FPA) in relation to land controlled by Bloor Homes in the southern part of the allocation site to the east of Stafford Road for the delivery of 135 dwellings and supporting infrastructure;

The three applications are underpinned by a shared Environmental Impact Assessment, other technical and environmental assessments, and this Design and Access Statement. The documents have addressed both the assessment of the site and the design of the development scheme on a comprehensive site wide basis.

This strategy will facilitate the multi-phased delivery of the site at the earliest opportunity, ensuring that market and affordable housing needs and demands are addressed in a timely manner within the plan period. It will also ensure that infrastructure delivery will be appropriately phased in line with residential delivery.

## Development Phasing

The likely phasing of the development scheme is outlined in the opposite figure.

The submission of a FPA by Bloor Homes will facilitate the early delivery of the area to the east of Stafford Road, using the existing highway infrastructure close to the existing built form of Penkridge and the ongoing development to the west of Stafford Road (Phase 1a).

This will be followed by delivery in the centre of the site focussed around Stafford Road (Phase 1b), which will be accessed via the new roundabout on Stafford Road, and will allow for the delivery of the community hub and school land at an appropriate stage (trigger to be confirmed) to support the formation of the new community, and the potential early delivery of the Specialists Older Persons Housing.

From these points, development will proceed towards the east, west and north of the site in a logical manner through to completion.

## Affordable Housing Delivery

Affordable housing will be provided in small clusters throughout the new neighbourhoods, with each application area accommodating a proportionate amount to address the Local Plan policy requirement.

## Infrastructure Delivery

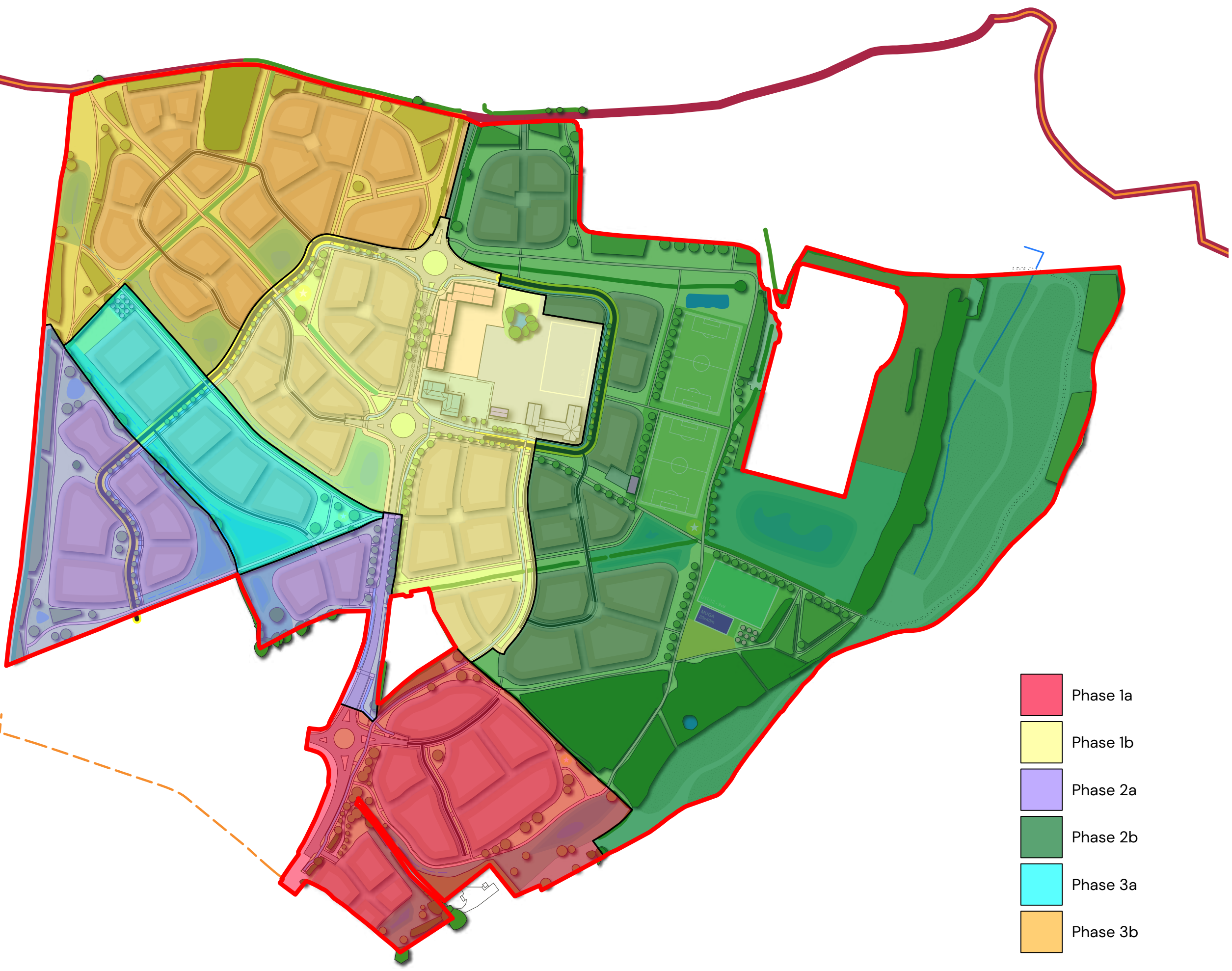
The co-ordinated and phased delivery of the infrastructure needed to support the delivery and on-going day to day needs of the new neighbourhood will be critical.

### *Access and Drainage Infrastructure*

The Masterplan has been designed to ensure that each development area (subject to its own planning application) can be appropriately accessed and drained. The infrastructure required to realise each area will, therefore, be delivered as an integral part of those development proposals.

Where the active travel (e.g. the strategic route) or internal highway links pass through different planning application areas, each applicant will be responsible for delivering the required infrastructure within their part of the site. The applicants will accept planning obligations to ensure that the required active travel links are connected in a timely manner.

- Phase 1a
- Phase 1b
- Phase 2a
- Phase 2b
- Phase 3a
- Phase 3b



### ***Public Open Space Provision***

The Masterplan has been designed to ensure that each application area provides the requisite amount of public open space needed to meet the Local Plan policy requirement as a minimum.

The delivery of the Riverside Community Park will form an integral part of the St Phillips OPA and Bloor Homes FPA.

The delivery and future management of the public open space will be secured by the Section 106 Agreement attached to the grant of each permission.

### ***First School and Sports Pitches***

These facilities form a key part of the proposed development scheme and are required to support the creation of a sustainable and vibrant community.

The proposed phasing outline above allows for their delivery at a relatively early stage in the site's development. Moreover, a mechanism has been agreed between Bloor Homes and St Philips to ensure that the necessary funding is also available at an early stage. The development cost and land value will be aggregated and then split proportionately between the applications, with the land value then discounted from the application that would provide the site for those facilities. The payment of the financial contributions and the transfer of serviced land to the relevant Council would then be secured by a Section 106 Agreement attached to each permission and once paid the developers should be free to proceed with delivery of the new homes without restriction.

### **Off-site community infrastructure enhancement / mitigation**

This is likely to include the enhancement of other tiers of education provision, health and community facilities, and will be funded by proportionate financial contributions secured by Section 106 Agreements attached to each planning permission.

A financial contribution to the Cannock Chase SAC recreation mitigation scheme will also be secured through the Section 106 agreements.

### ***Off-site highways, active travel & public transport infrastructure improvements***

Required offsite highway/travel infrastructure improvements can be delivered by the Highways Authority directly. In that case, proportionate financial contributions to the total cost of those works would be secured by the Section 106 Agreements attached to the permissions.

If the developers are required to deliver the necessary works themselves, then the total required works would be packaged to allow the developer for each application area to deliver a proportionate element of any required improvement scheme, phased appropriately.









# MASTERPLAN PRINCIPLES



# Community

*An attractive and vibrant neighbourhood will be created, with a range of homes for all needs, local facilities, sustainable transport opportunities and open green spaces. This will provide the foundations for a healthy and happy new community that is sensitively integrated into its environment.*

## RESIDENTIAL DEVELOPMENT

The development will deliver up to 1,100 new dwellings including Specialist Older People's Housing (SOPH). The new homes will be sustainably designed to achieve low carbon living and built to last.

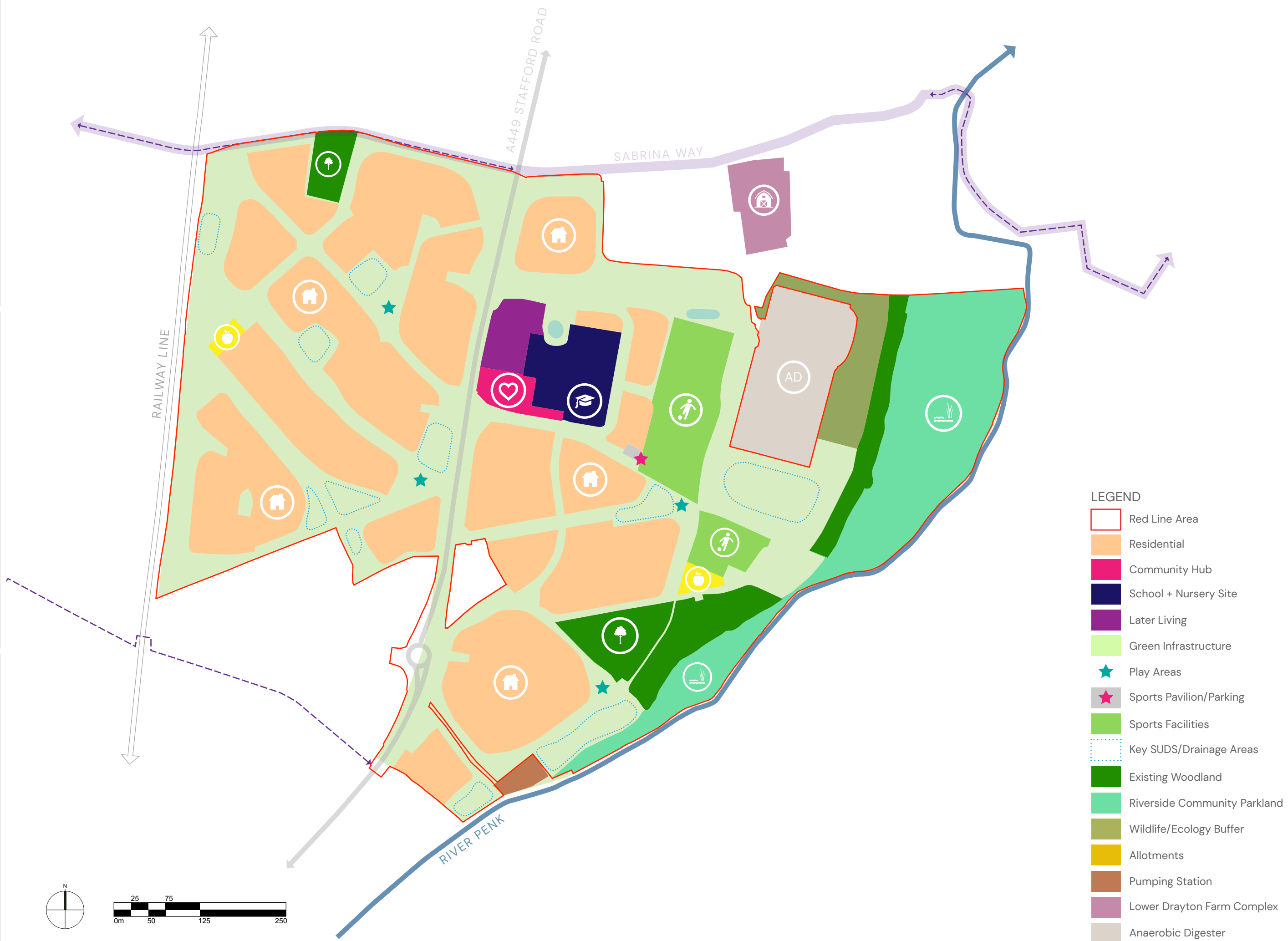
Residential development will be served from the internal road hierarchy, and will be set out in perimeter blocks. It will, therefore, positively address the public realm and areas of open green space, whilst also securing private spaces to the rear.

The housing provision will begin to address the acute housing need in the area and, in turn, will realise a number of significant socio-economic benefits.

The neighbourhood will include a varied mix and range of house types to reflect housing needs and demand, from starter homes through to larger family homes. This will include 40% affordable housing.

A minimum of 40 units of SOPH will be provided. It is not possible to determine at this time whether that will be later living accommodation (C3 use) or whether it will include an element of care (C2 use). It will, however, be centrally located close to the community hub. The Masterplan has therefore designated an area of circa 0.7ha to the immediate north of the hub where the SOPH will positively address the Stafford Road, but can be accessed from within the development.







## THE COMMUNITY HUB

The community hub is a key element of the new neighbourhood. Its location, content and form have been carefully considered through the masterplanning process to ensure that it meets the needs and demands of the new community and is accessible to all by sustainable modes of travel.

The Masterplan therefore proposes to co-locate the local centre uses and the first school and nursery site. This will provide a hub of life and activity at the centre of the development, which will form a focal point for the new neighbourhood based around a shared public realm. The “plaza” will provide an attractive landscaped setting to the key buildings and will be an important facility in its own right as a place to meet and socialise.

The combination of uses will be mutually supportive, ensuring the future vitality and viability of the local centre. The school will also benefit from the use of the public realm and the local centre car park at drop off and pick up times.

The location of the community hub at the centre of the development, adjacent to the Stafford Road, will ensure that residents in all parts of the site have good access to the services and facilities that are on offer therein. It is the optimal solution in that regard as its location at the confluence of the public transport and active travel routes throughout the site will provide direct, safe and convenient access for all residents wherever they reside in the neighbourhood. This will reduce the need to travel by car and promote active travel.

The Masterplan proposes to locate the community hub to the east of Stafford Road, where it can be served directly by the proposed strategic active travel route that connects the new neighbourhood to the village centre. This location also benefits from a direct relationship with the sports and play facilities and community park that will be located a short walk away along the green spine to the east. Together, these areas will provide an attractive and accessible health and wellbeing area at the heart of the development, which will be a significant benefit to the community.

### The community hub will include:

- **A Local Centre**

Stakeholder engagement has highlighted that the local community would welcome some retail provision to provide variety to the existing retail offer in the village. However, it is also important that the new provision does not directly compete with the village centre offer. A retail unit of up to 500m<sup>2</sup> is, therefore, proposed to accommodate a range of small shops or a larger single food store.

The local centre will also accommodate other services to meet the day-to-day needs of residents, which may include a hot food takeaway, cafe, pub / bar, community work space or other commercial services such as hairdressers and the like.

Precisely what will be provided cannot be determined at this time, but the total floorspace will be up to 1,000m<sup>2</sup> (including the 500m<sup>2</sup> of the retail uses).

- **Community Space**

A dedicated community building is not proposed. This reflects the feedback received during the stakeholder engagement, where respondents instead favoured investing in the maintenance of the existing community buildings in the village. However, a small community space could be provided within the local centre building to serve specific local needs in the neighbourhood.

- **A first school and nursery site**

The community hub will accommodate a 1.5ha new first school and nursery site, as required by the emerging allocation policy. Following engagement with the County Council, it is located to the east of the local centre to ensure that there are no traffic conflicts on the A449.

The school building should be positioned to positively address the community hub in a prominent location. The school building can then also provide an attractive and secure boundary to the remainder of the site to its rear.

- **Public Realm**

A plaza will provide an attractive setting for the community hub and a place to meet and interact at the heart of the development. It will accommodate the interface between the active travel and public transport infrastructure and the uses within the local centre, and will also provide an appropriate level of car parking.

## PUBLIC OPEN SPACE

The new neighbourhood will accommodate a generous quantum and variety of public open space within its Green Infrastructure framework that significantly exceeds the standards set out in the extant and emerging policies.

- **A Riverside Community Park**

A large area of natural open space adjacent to the River Penk will be delivered as a Community Park, providing opportunities for informal recreation and dog walking.

- **Green Corridors**

A network of green corridors will provide an attractive landscaped setting and easily accessible doorstep open space for the new homes, while also accommodating footpaths and cycleways, planting and sustainable drainage features (SuDS). This will create a strong sense of place, ensure that the natural environment is part of the community's everyday life, and provide opportunities to meet and play, in turn strengthening social cohesion.

- **Food Growing**

The Masterplan includes two formal allotment areas, located near to the eastern and western boundaries of the site to reflect feedback in the stakeholder engagement process. Green corridors could also incorporate community grow boxes to provide an opportunity to socialise and foster ownership. Smaller orchards will also be distributed sporadically around the site.

- **Children's Play**

The development will provide a range of strategically placed play areas that are accessible to all. Opportunities for informal play will also be provided by delivering 'play on the way' facilities within the green corridors. This will allow children to interact with the surrounding natural environment and bring life to the edge of residential areas.

- **Sports Pitches & Recreation Facilities**

The development will provide well-located sports pitches and recreation facilities to meet local needs.

Sports provision will be provided within a 2.94ha area that has been identified on the flatter land to the west and south of the AD plant, close to the school and community hub. The Masterplan includes landscaping that will provide an appropriate buffer between the two.

There was no clear consensus from either the Council's Playing Pitch Strategy or the stakeholder engagement as to the types of pitches required, although there appeared to be a general preference for adult and youth football pitches. Therefore, the Masterplan proposes 2no. full-sized football pitches, a U11/12 9v9 pitch, and a Multi-Use Games Area (MUGA). A further youth pitch will be provided in the school site.

The playing pitches and MUGA, together with the allotments and orchards, will form part of an open space hub for the new community and the gateway to the proposed community park alongside the River Penk to the east.





# Access & Movement

*An emphasis on sustainable travel options will help minimise use of the private car. A network of greenways, cycleways and footpaths will take pressure off local roads and contribute to residents' health and well-being:*

## ACTIVE TRAVEL

Active travel within the site will be encouraged and facilitated through the provision of dedicated routes and green corridors.

It is important that the new neighbourhood has a strategic active travel route through the site in order to divert pedestrian and cycle traffic away from Stafford Road. This will be to LTN 1/20 standard, and will create an attractive and convenient alternative link between the community hubs, open spaces and the village centre to the south without the use of the car.

Elsewhere, a hierarchy of active travel routes will permeate the site, notably alongside the primary streets and through the green corridors. They will provide direct, safe and attractive connections to the community and open space hubs from all parts of the site. Safe, signalised crossings over the A449 will be provided on the key desire lines,

and supplemented by additional crossing facilities elsewhere.

The wider connected network of routes around the site, which connect to the surrounding public right of way network, will also provide a multitude of options for exercise and social gathering.

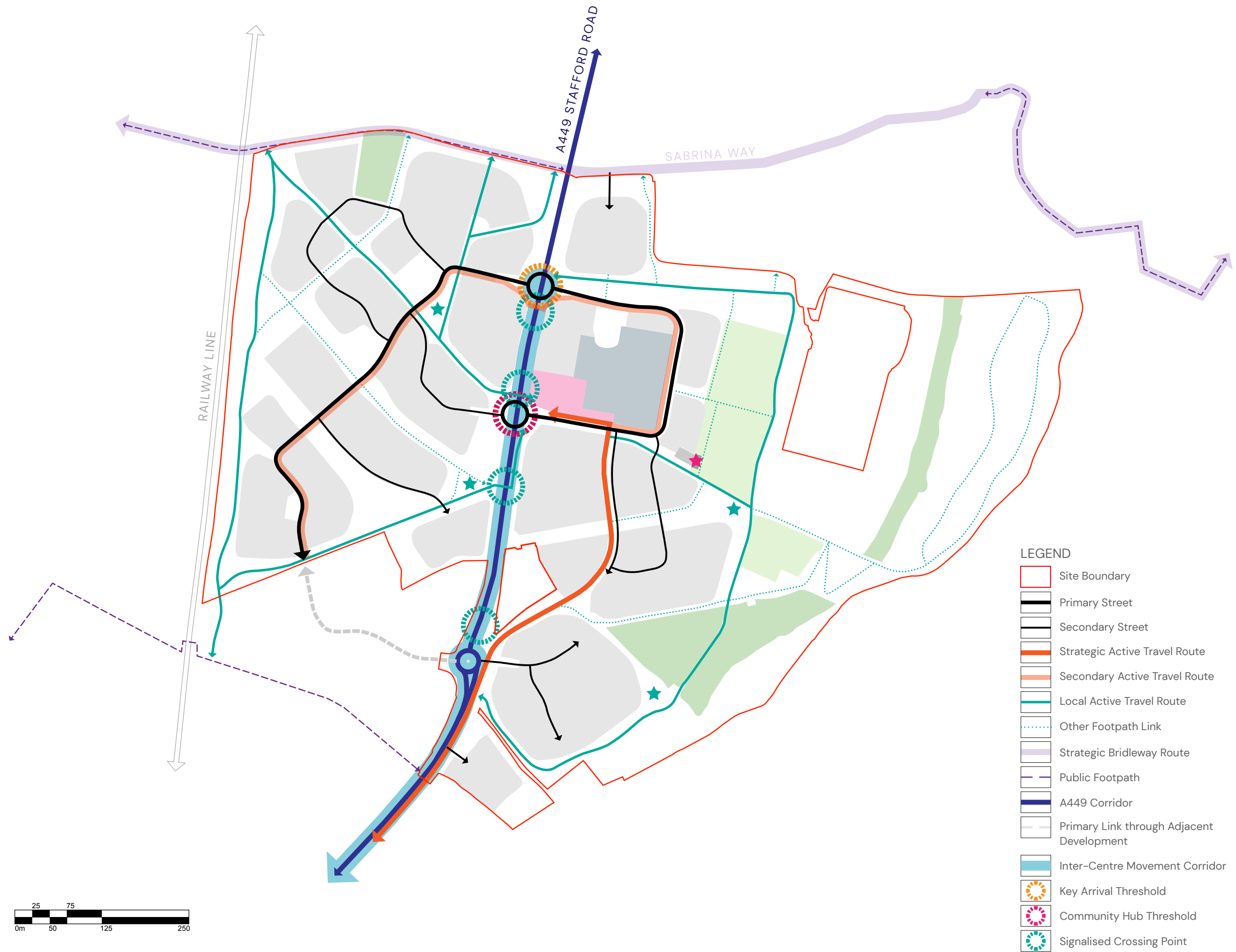
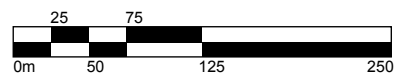
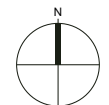
## MOBILITY HUB

Mobility hubs are accessible spaces where public, shared, and active forms of travel are co-located alongside public realm and community facilities to facilitate safe, convenient and active journeys.

They are best located at key transport corridors to integrate with existing public transport routes, and can become a focal point for communities where they are provided alongside key services and facilities. A mobility hub is therefore proposed within the community hub adjacent to Stafford Road, which will remain the key bus corridor.

Careful consideration has also been given to how the active travel routes that will permeate through the development will connect with the mobility hub. The aim is to facilitate safe, direct and convenient sustainable travel beyond the new neighbourhood by facilitating seamless multi-modal journeys. That will, therefore, reduce the dependence on car use.







## SITE ACCESS

Access to the development areas will principally be provided from Stafford Road via two new four arm roundabouts that will provide access to the east and west development parcels. Provision of a fourth arm to the existing roundabout which serves the Bloor Homes development that is currently being delivered will provide access to the southern part of the site.

The northern roundabout will act as a gateway feature into the village. It will mark the new northern settlement edge and provide an effective speed reduction measure to assist in the transition from 60mph to 40mph, and then to 30mph.

A second roundabout will be provided at the centre of the site, and will serve both residential development and the community / green space hubs that are located to the east of Stafford Road.

The development areas to the east and west of Stafford Road will be served via a looped internal arrangement of tree lined primary streets linking the roundabout accesses.

A permeable internal road network established by a clear street hierarchy, as defined by the coding principles, will provide access to individual development blocks.

## STAFFORD ROAD CORRIDOR

There is a wide highway corridor as Stafford Road passes through the site. This provides the opportunity for its enhancement to better meet the needs of all users, whilst ensuring that it integrates rather than separates the new neighbourhood.

As the cross-section below shows, the road carriageways will be reduced between the two new roundabouts, albeit with a central refuse to accommodate larger vehicles when needed. 3m shared pedestrian and cycle paths will be provided to either side of the road, together with wide verges that will accommodate street trees and, where required, bus stops and service lay-bys for the local centre.

Dedicated crossing points will be provided to aid movements across Stafford Road. This will include signalised crossing points at key desire lines to

ensure safe and convenient active travel movements between the development areas.

The built form along the Stafford Road corridor will be informed by the principles set out in the coding (as summarised later on in this document), and will respond to Stafford Road to create a sense of place.

Access to the properties along Stafford Road will predominantly be from within the development areas. There will, therefore, be no direct access to properties from Stafford Road, except for the small area located to the south of the central roundabout.

The proposed approach to Stafford Road will, therefore, balance the functionality of the road as a key transport route with the delivery of new vehicular accesses, safe and convenient pedestrian and cyclist crossings, and a development with a strong sense of place.



## OFF-SITE HIGHWAYS IMPROVEMENTS

The Transport Assessment (TA) prepared by PJA has set out opportunities to improve cycling and walking infrastructure off-site, in order to promote active forms of travel. The TA outlines a set of design recommendations, including:

- A449 corridor – two-way segregated cycleway between the proposed development and existing infrastructure in the vicinity of the village centre;
- Route to Penkrige Middle School – the TA recommends features to slow vehicle movements, improving the environment for those walking and cycling to school;
- Route to Wolgarston High School – improvements to reduce vehicle speeds, in order to improve the walking and cycling environment;
- Route to Penkrige Rail Station – proposed that cyclists are accommodated in the carriageway. Potential for one-way system on Pinfold Lane, Church Road and St Michaels Square to further reduce traffic volumes.

The nature of those improvements will be considered through the application process in consultation with the Highway Authority.





# Green Infrastructure

*An expansive green & blue infrastructure network will ensure that the natural environment is part of the community's everyday life, providing opportunities to meet and play, strengthening social connections and encouraging active travel, as well as providing landscape and ecological enhancements.*

This will have its foundations in the existing fabric of the site, working with the underlying topography and retaining and enhancing the site's natural assets, including hedgerows, trees, woodlands, ponds and drainage corridors.

The key Green Infrastructure principles embedded in the emerging masterplan include:

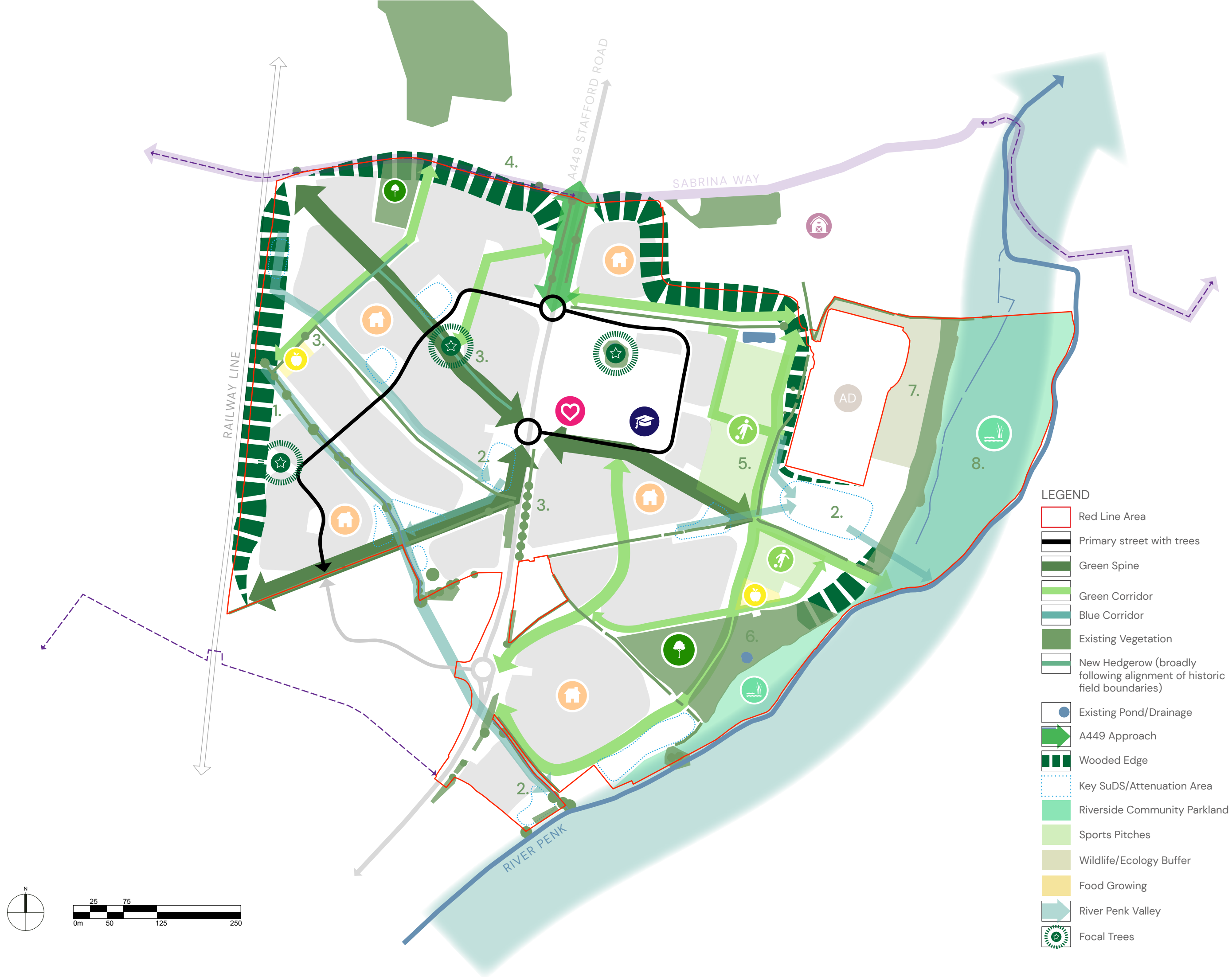
1. A wooded green corridor along the western boundary provides a landscape buffer between the proposed development and adjacent railway line and incorporates and frames views to the church tower in Penkridge.
2. SuDS provision will reflect the proposed drainage strategy. The features will respond to the underlying topography and be arranged around existing

hedgerow and drainage corridors – reflecting the arrangements in the adjacent development to the south.

3. Retained existing hedgerows, and new hedgerows based on historic field boundaries, will provide a framework for the network of green corridors across the site, linking housing areas with key locations and facilities within and surrounding the development. Key existing vegetation will be retained as focal points within the layout.
4. The northern edge will be strengthened with tree planting, reflecting the character of the surrounding woodland blocks and providing a buffer to the strategic bridleway corridor. It will also help to soften the impact of the development as it extends northwards up the slope.
5. Grassed pitches will be located on the flat land to the south and west of the AD plant, and will form part of an open space hub within this area of the site.
6. The existing woodland will be retained within the site, providing recreational and ecological benefits close to the proposed development.
7. A more remote ecological area will provide a buffer around the north-eastern edge of the AD plant, close to established tree and woodland belts.
8. A riverside community park will have an ecological focus. It will incorporate the existing flood zones associated with the River Penk while maintaining the open, linear character of the river corridor. Strategic tree planting around the periphery will help to screen views of the nearby motorway.











# PLACEMAKING CODE



# Design Strategy

The key objective of the design strategy is to ensure that the development is recognisable as an extension to Penkrige, but with its own distinctive individuality and quality. It has, therefore, been significantly influenced by the assessment of the site and its surrounding area.

The design strategy will create a strong, cohesive identity for the new neighbourhood, bound together by the extensive Green Infrastructure framework that will frame and permeate the development, but with subtle changes to the built character in different areas to reflect the varying nature of the site and its context.

Asympathetic arrangement of built form, constructed from a robust palette of materials, should relate well to the key movement routes, adjacent public realm and open spaces and existing natural features. With this in mind, the development should take its cues from the built form and materials seen within the locality, but present a 21st Century character in a respectful manner with appropriate detailing (as advocated by SSC's Design Guide SPD).

## APPROACH TO PLACEMAKING

The NPPF (September 2023) states that *"the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Being clear about design expectations, and how these will be tested, is essential for achieving this."*

It continues to outline that design guides/codes can *"provide maximum clarity about design expectations at an early stage" and "a local framework for creating beautiful and distinctive places with a consistent and high quality standard of design."*

The National Design Guide and National Model Design Code also provide guidance for *"well-designed places that are beautiful, healthy, greener, enduring and successful"* and should be read alongside the NPPF and other design guides and tools.

This part of the document focusses on good place making design principles that link the Vision and Masterplan for the development to the detailed design of the final scheme proposals, to ensure that they reflect the key development parameters and overarching urban and landscape design strategies.

There is a balance between prescription and flexibility to facilitate individual interpretation and creativity

whilst maintaining the objectives for the new neighbourhood. This is consistent with the Planning Practice Guidance note on Design which states that *"a (design) code can be a way of simplifying the processes associated with new development to give more certainty to all those involved and help to make high quality places" and explains that "to promote speed of implementation, avoid stifling responsible innovation and provide flexibility, design codes should wherever possible avoid overly prescriptive detail and encourage sense of place and variety"*.

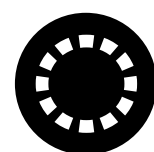
A Place Making Plan, based on the Masterplan and a number of clearly defined design parameters and principles, has been produced and will be used to help plan and guide the new development, to achieve a high standard of design and create a strong sense of place, realising the vision as set out in the preceding sections.

The Place Making Plan is not itself a "fixed" development plan; that will be resolved through the detailed design process at the Reserved Matters stage. It does, however, clearly identify the more detailed placemaking components of the proposed development and where they should be applied within the detailed schemes. The highlighted coding principles are mandatory and any departure at the reserved matters application stage will require justification. Illustrative material and design precedents are provided to show indicatively how the coding principles can be fulfilled.



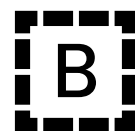
## UPPER PENKRIDGE

- More contemporary style.
- Greater sense of formality.
- Selective number of housetypes and simple building forms.
- Greater use of repeating elements and symmetry.
- Use of timber effect cladding to reflect nearby woodland.
- Features to take advantage of views from higher ground and across open space corridors.



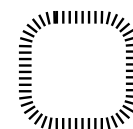
## COMMUNITY HUB

- Distinctive identity.
- Buildings based on contemporary interpretation of traditional farm buildings – reflecting origins of local settlements.
- Simple building forms.
- Distinctive facade treatments – such as cladding, vintage brick and fenestration.
- Focal public realm accommodating a range of local community uses.



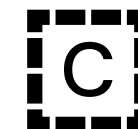
## DRAYTON CHASE

- Semi-formal character with variety of housetypes but some use of symmetry and repeating elements.
- Reflects character established in adjacent development with some features echoing the local vernacular.
- Emphasis on green corridors and SuDS – based on existing field boundaries and drainage features.



## STAFFORD ROAD

- Landmark buildings along northern edge to denote entrance into Penkridge – informed by existing precedents along Stafford Road and in the village centre.
- Creates sense of formality and arrival.
- Take contemporary cues from existing A449 frontage within the village – including use of render, double height bay windows and timber frame detailing.

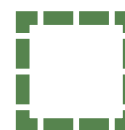


## LOWER PENKSIDE

- Buildings take cues from traditional vernacular & nearby heritage assets.
- Informal character with a variety of housetypes unified by a common building line.
- Buildings orientated to take advantage of views over the river and towards Cannock Chase.

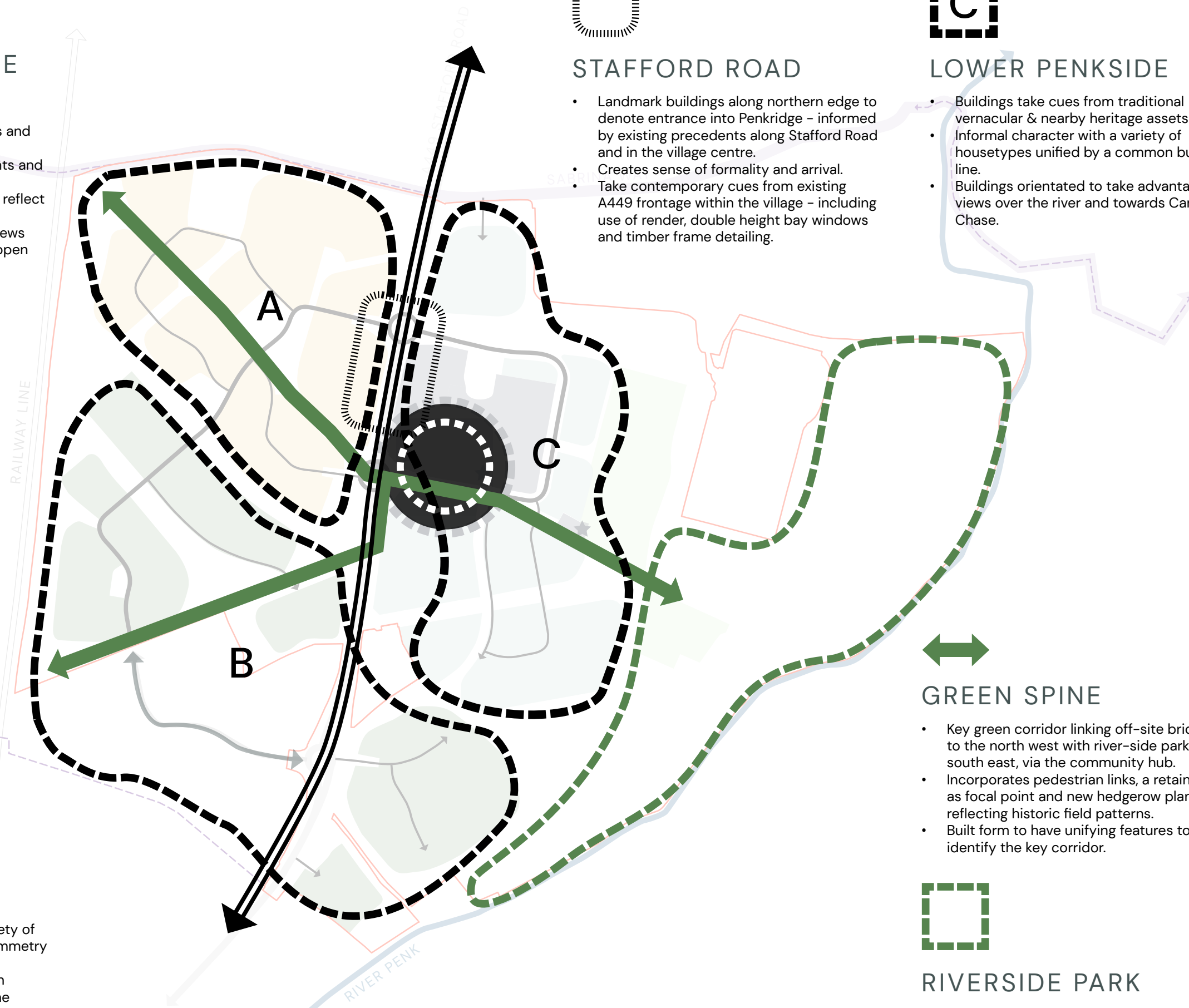
## GREEN SPINE

- Key green corridor linking off-site bridleway to the north west with river-side park in the south east, via the community hub.
- Incorporates pedestrian links, a retained tree as focal point and new hedgerow planting reflecting historic field patterns.
- Built form to have unifying features to identify the key corridor.



## RIVERSIDE PARK

- Ecological grassland emphasis on lower ground close to river.
- Community open space uses (food growing, sports, informal recreation) on plateau area.



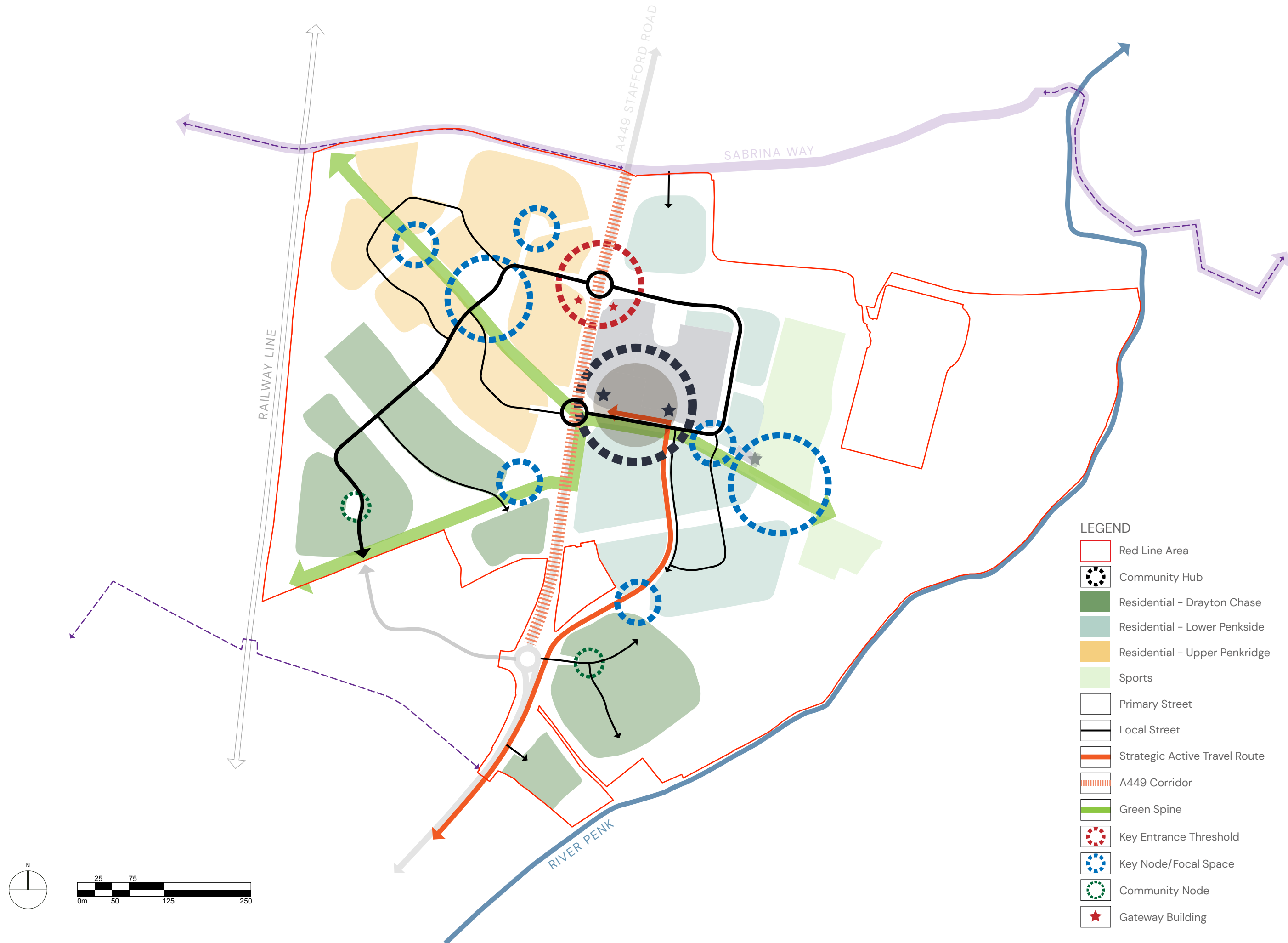


# The Placemaking Plan

The Placemaking Plan (shown right) identifies some of the key components of the Masterplan, including:

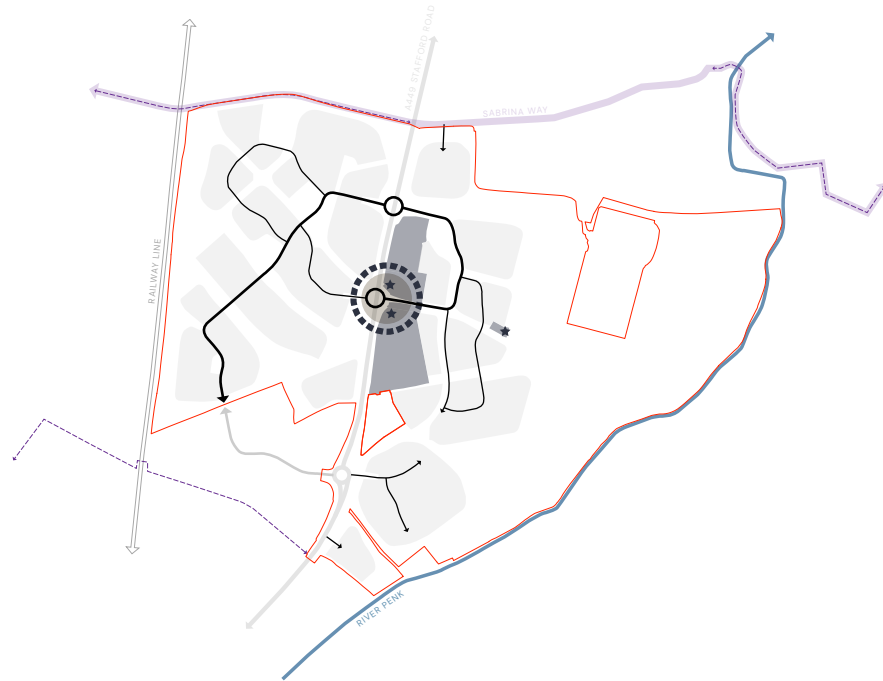
- Character Areas;
  - Lower Penkside
  - Drayton Chase
  - Upper Penkridge
- Community Hub;
- Stafford Road Corridor (A449);
- Key Movement Hierarchy;
- Green Spine;
- Key Entrance Threshold;
- Gateway Buildings; and
- Key Nodes/ Focal Spaces.

Compliance Statements will be required to accompany future Reserved Matters applications that make reference to the way in which all Coding Principles have been addressed and how the respective requirements have been met.





# Community Hub



## THE VISION

Many traditional South Staffordshire villages began as small farming communities, and grew around a church or a market place. The community hub within Penkridge North will fulfil a similar function, providing a focal point for the new community at a scale that does not undermine the importance of the historic centre and range of facilities within Penkridge. In a nod to its rural context, key buildings within the community hub, including the school, a small number of local shops and a flexible community space, will reflect traditional farm buildings in a more contemporary way, using simple forms, roof profiles and materials to create a distinctive identity at the heart of the new development. These will enclose a high quality public space, reflecting the farm courtyard and defined by distinctive public realm and landscaping, that will incorporate areas to meet and sit out, safe drop-off and collection spaces for the school and nursery, a modest amount of well integrated parking and a mobility hub.

It will be well connected to the surrounding residences, with an emphasis on active travel routes radiating outwards from the centre. It will also maintain strong links with the main village centre, with regular bus services and cycle routes along the A449 corridor providing access to the railway station and existing services within Penkridge.

LOCAL RETAIL,  
COMMERCIAL, FOOD  
& DRINK WITH  
COMMUNITY USE  
(IF REQUIRED)



FIRST  
SCHOOL

NURSERY



SPECIALIST  
OLDER  
PEOPLES  
HOUSING  
(MINIMUM OF  
40 UNITS)



PUBLIC PLAZA,  
PARKING &  
MOBILITY HUB





## CODING PRINCIPLES

### Building Form & Layout

- Key buildings will reflect traditional farm buildings in a contemporary way - using simple forms, roof profiles and materials to create a distinctive identity that reflects their status within the community hub.
- Buildings should be arranged so that positive active frontages address the central public space.
- Linked forms should be used where possible - to create continuity and containment around the central public space.
- Buildings within the community hub should be flexible in their design to accommodate other uses and adapt to changes in market demand.
- Residential, office accommodation and/or other compatible uses should be accommodated above the ground floor retail/commercial to promote activity throughout the day and provide natural surveillance over the central public space.
- The architectural language and massing of the individual buildings surrounding the central plaza should also be considered collectively to ensure that they complement each other and create a sense of place.

### Building Heights

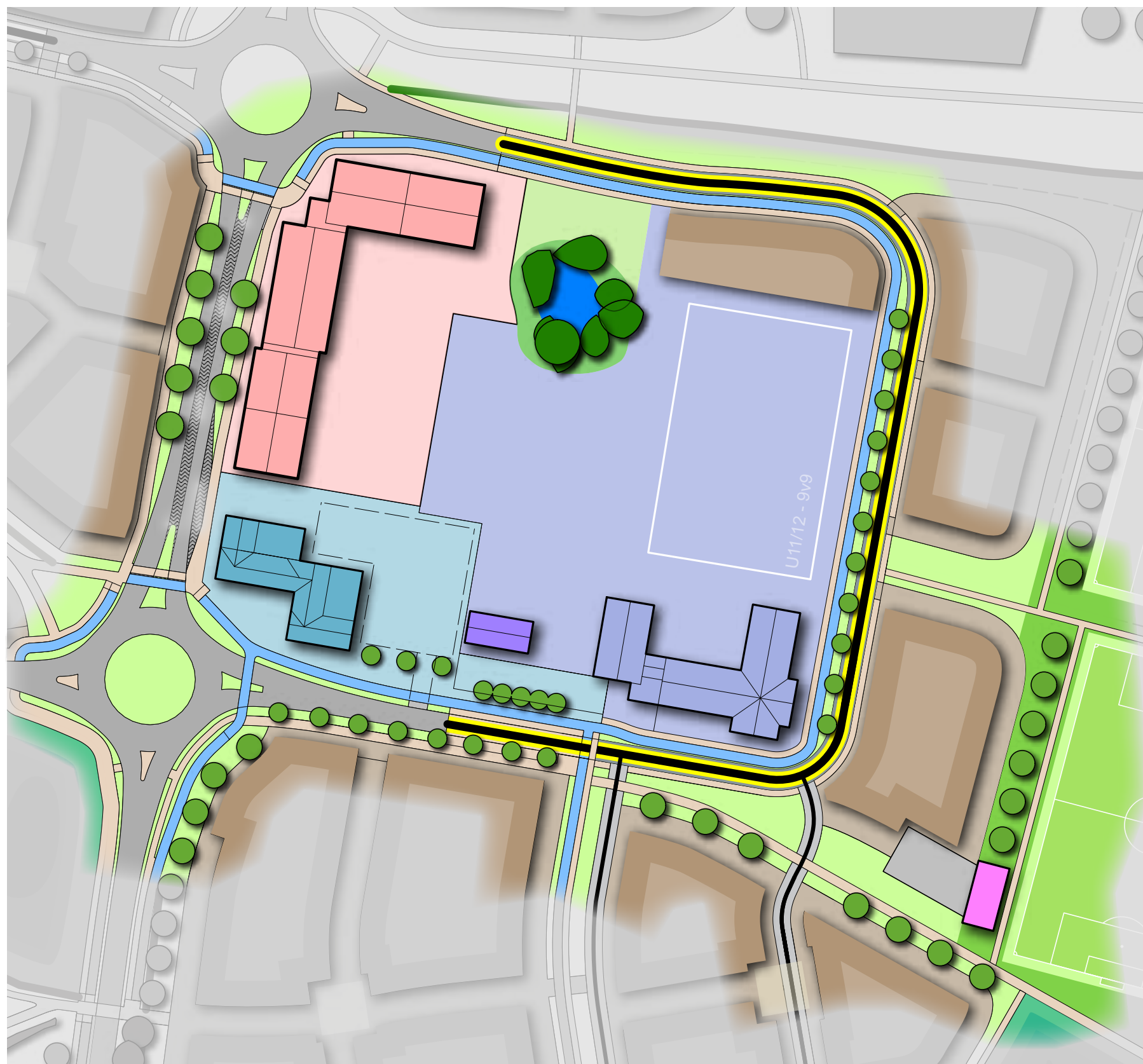
- Generally larger buildings when compared with surrounding housing - creating a focal point at the heart of the development.
- Typically the equivalent of around 3 to 4 residential storeys.
- Ground floors in mixed-use blocks to be taller so as to provide the flexibility to accommodate a variety of land uses.
- Key buildings or entrances may be denoted by an increase in height or scale.

### Architectural Treatment

- Contemporary interpretation of traditional farm vernacular expressed in simple forms and detailing.
- Bespoke architectural response for each building appropriate to its functional and operational requirements.
- Some unifying themes/components referencing local detailing may be useful to help add to the sense of place.
- Use of bespoke fenestrations.

### Materials

- Appropriate to the contemporary farm vernacular theme.
- Timber or timber style cladding in dark painted or natural finishes.
- Red brick to match traditional local palette
- Use of buff or sand coloured brick may be appropriate in this location - reflecting its traditional use locally in civic buildings or churches and providing a contrast with the red brick used in the surrounding residential areas.
- White or coloured render
- Timber framing.
- Greater use of glazing.





# Key Buildings & Plaza



## KEY DESIGN PRINCIPLES

### The School & Nursery

- Positioned within the site to the east of the retail and commercial uses.
- Simplistic building forms, taking cues from the traditional farm building vernacular in a contemporary way, and using high quality materials and finishes.
- The main school building will act as a local landmark, creating a distinctive focal point befitting its prominent location and role within the community.
- Positioned to positively address the eastern extent of the central plaza area, the mobility hub and the surrounding movement routes.
- Will include a well appointed and easily identifiable main entrance or reception area. This may be articulated by colour, material changes or architectural features.
- The school frontage and arrival space will incorporate high quality landscaping, tree planting and seating areas that complement the central plaza area and allow the spaces to be read 'as one'. This will help to calm traffic and enhance interconnectivity while creating a strong sense of identity.
- To encourage use of other active travel and public transport modes, drop-off parking will be kept to a minimum and integrated into the public plaza space.
- Staff parking and servicing will be accommodated to the rear and accessed via a gated entrance off a secondary street – away from the public plaza.
- For safeguarding, public access to the site will be controlled and restricted to the area around the central plaza.
- Elsewhere, the building form and secure boundaries will clearly define the thresholds of the site, with playground areas and outdoor classroom spaces located to the rear of the building, away from public areas.
- Opportunities to provide shared community use of facilities, such as the hall or sports pitch, should be explored. This may include the creative design of internal defensible boundaries to allow some parts of the site to remain open while the core school buildings and spaces remain secured.

## KEY FEATURES

- 1 Separate lay-bys on the A449 for a bus stop and for servicing the retail/commercial units from the road.
- 2 Main retail unit within a landmark building helps to signpost the community hub and create a focal point for the development.
- 3 Distinctive entrance space for main retail unit – including feature paving, seating and landscaping.
- 4 Central plaza area with distinctive surface treatments and landscaping linking key community facilities and car park. Will accommodate flexible areas for outdoor seating, cycle parking and pop-ups.
- 5 Central parking area provides access to nursery and retail/commercial units and option for drop off at the school. Also accommodates mobility hub functions.
- 6 School arrival/collection space provides a public threshold off the central plaza and allows access to hall and school reception.
- 7 Clearly defined building entrance into school. Reception and administrative areas located around the main entrance to provide a buffer between the internal educational spaces and the adjacent public plaza and active travel routes.
- 8 Playground and outdoor activity areas secured within the site to the rear of the main building.
- 9 School hall arranged to allow for flexible community use if required.
- 10 Internal fence boundaries allow school site to remain secure while potentially allowing controlled use of external parts of the site (junior pitch/parking area) by public on evenings and weekends.
- 11 Separate access to staff car park/service areas from side road. May facilitate controlled public access if required.
- 12 Adjacent housing wraps around the school boundary to the north.
- 13 Housing frontage to create a strong sense of enclosure along the southern edge, with linked properties set close to the back of the footpath and parking accommodated to the rear.
- 14 Sports pavilion with parking.





## Retail & Mixed Use

- Linked form comprising a landmark focal building at the A449 junction and a linear block extending eastwards along the northern edge of the central plaza.
- Ground floor units have extended heights so that they may accommodate a variety of uses.
- Ground floor uses may include a cafe, a small convenience store, local retail, offices or commercial.
- Residential, office accommodation and/or other compatible uses should be accommodated on upper floors to promote activity throughout the day and provide natural surveillance over the central plaza.
- Service areas should be provided to the rear and accessed from side streets rather than through the main frontage.
- The landmark building on the corner with the A449 may be serviced from a lay-by on the main road.

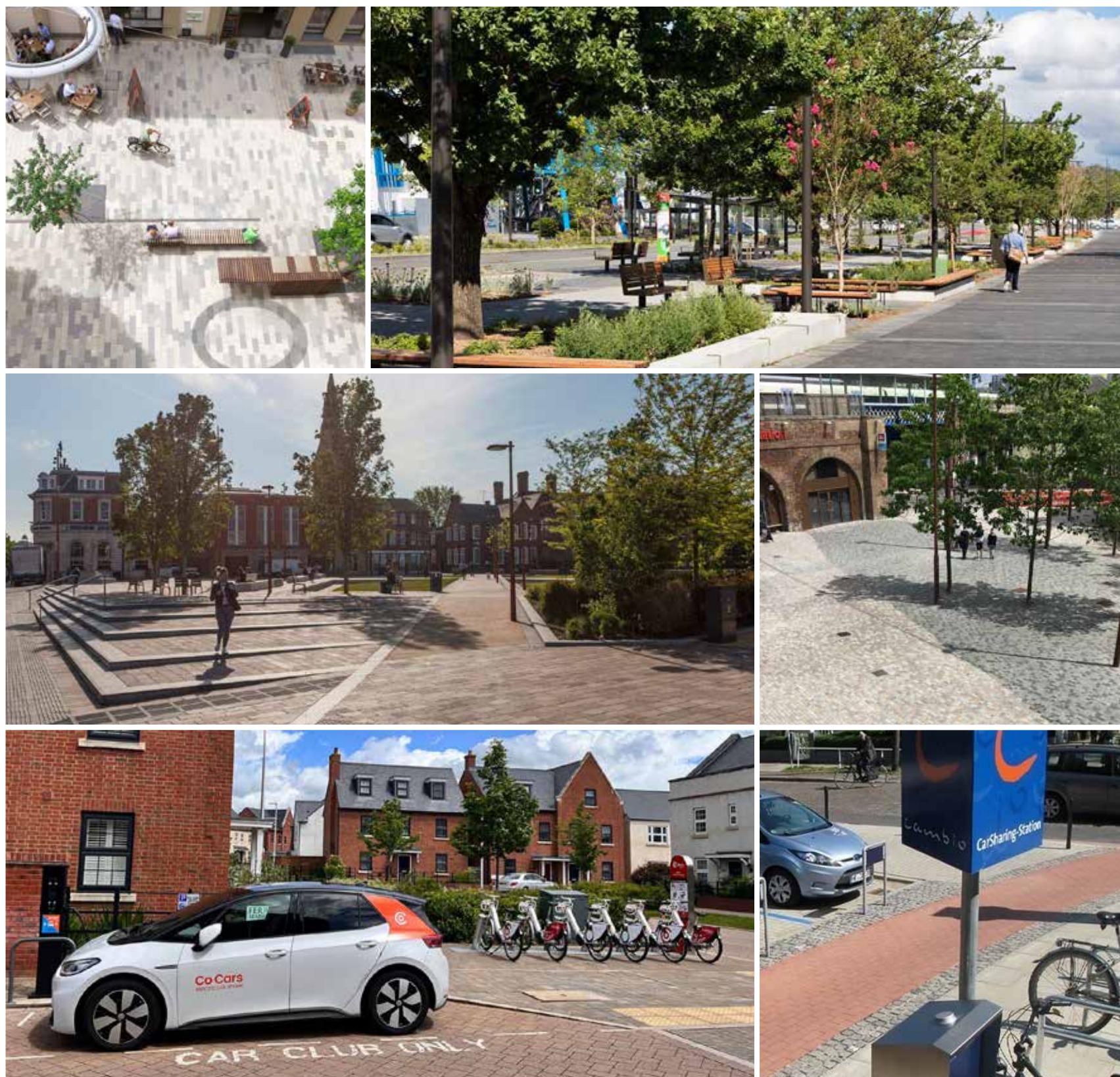
## Later Living

- A minimum of 40 units of specialist older people's housing located close to the community hub, to the rear of the retail and mixed use component.
- Mix and type of provision to depend on operator – may be flatted development or mix of flats and small individual properties.
- Buildings to be arranged as a perimeter block with habitable rooms and (if appropriate) doorways looking out onto surrounding streets.
- Buildings to enclose an internal courtyard/garden space which may accommodate shared and semi-private amenity spaces, parking and servicing.
- Controlled gated access to internal courts to be provided for security.



Examples of contemporary community/retail buildings with a farm aesthetic





Examples of public realm and mobility hub

## Central Plaza

- High quality soft and hard landscaping with a common palette to bring together the surrounding land uses and create a sense of place.
- Careful integration of pedestrian and cycle routes, car parking and access/service requirements.
- Distinctive tree species and/or forms.
- Use of trees and canopies to provide areas of shade and shelter.
- Provision of some areas of multi-functional hard standing to accommodate a range of casual uses (such as street performance, pop-ups, exhibition space, etc).
- Areas and seating for resting and meeting to promote social interaction.
- Paved margins to provide spill-out spaces or outdoor seating areas for ground floor uses.

## Mobility Hub

- Flexible provision that may grow organically as different modal shifts and demands evolve.
- Bus lay-by with shelter and seating allows people to easily access the village centre and train station via a regular bus service.
- Secure bike parking within close proximity of the bus stop.
- Potential real-time updates on bus services so people know when the bus is arriving.
- Potential for nearby delivery collection points/lockers within easy reach of the bus stop – either well overlooked within the public realm or within nearby retail or commercial.
- Potential for car club, e-bike or scooter hire to be located within the mobility hub, where it can be easily accessed by the surrounding community.



# Drayton Chase



## THE VISION

This character area is located in the southern part of the site on both sides of Stafford Road. The area is structured by retained existing hedgerows, watercourses and woodland. It will feel like an extension of the new development being constructed to the immediate south, emphasised by direct connections provided by the shared use of the roundabout on Stafford Road, secondary streets and the network of footpath and cycle routes.

The built character will follow the precedents set by the adjacent development, with 2 storey red brick housing fronting out onto generous linear green corridors that follow the existing field pattern and accommodate SuDS features and retained hedgerows. While there will be a variation in the housing, some repeating elements and symmetrical groupings will add a sense of formality in key areas. The buildings will reflect some of the vernacular features established in the new development, including the use of arched porches or lintels above doorways, painted render, casement windows or exposed timber gables on key buildings. Some of these features can also be seen in the traditional vernacular within Penkridge.

As this area of the site is more enclosed and inward-looking, the network of green spaces will be an important focal point for the surrounding housing.



Existing development to the south west - which will inform the character of this area



Examples of vernacular features from within Penkridge



Vignette (1:500)



Variety of larger detached and semi-detached units define primary street with a consistent building line running parallel to the street and parking recessed to the side or within parking courts to the rear

Repeating and symmetrical elements on key frontage (overlooking focal open space) to give a more semi-formal feel

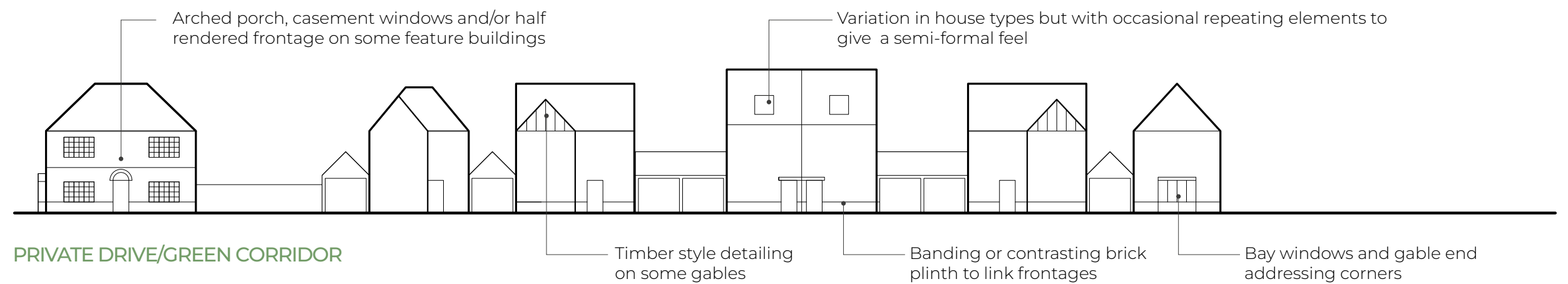
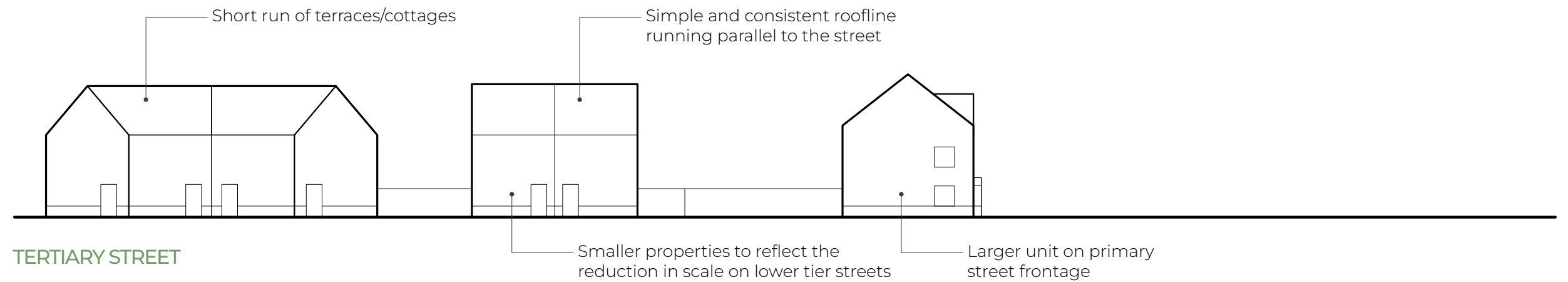
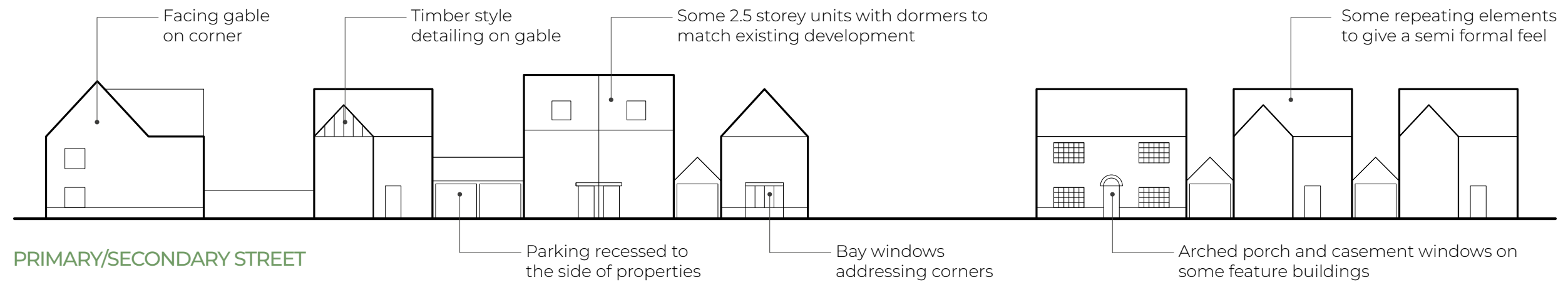
More intimate character within the tertiary street with smaller scale properties (including short runs of terraced housing), landscaping and frontage parking

Buildings arranged to positively address vistas and focal open space

Variety of predominantly detached housetypes fronting on to an adjacent green corridor and united by a common building line running broadly parallel with the private drive



## Typical Frontage Elevations (1:300)



# DRAYTON CHASE CODING PRINCIPLES

## Building Form

- In keeping with the character established by the existing housing development, and with a nod to the traditional vernacular there will be noticeable variation in the number and application of house-types across all street typologies, but with occasional groupings and key frontages displaying repeating and symmetrical elements to give a more **semi-formal** feel.
- On **primary streets** there will be no parking at the front of properties and on **secondary streets** it will be discouraged. This will ensure that the frontage has a strong visual relationship with the street. Unit types will mainly be detached or semi-detached properties along these streets so that parking can be recessed on driveways to the side or to the rear. This may include the use of drive under/drive through arrangements with accommodation above to create linked frontages.
- To reflect the more intimate, small scale character associated with **tertiary streets** there is more scope for short runs of terraced housing, flats over garages (FOGs) or linked cottages alongside frontage parking, either within curtilage, on-street or within small parking court areas. Within these streets there is scope for some small clusters of similar housetypes, giving a semi-formal feel.
- **Private drives** will mainly be located alongside open spaces. Functional restrictions on the number of properties served mean unit types will tend towards larger detached and semi-detached properties with parking either to the side or to the front.

## Building Line

- While there will be some variation in the appearance and arrangement of house-types within this character area, the building line, in tandem with the parking arrangements, will help to reinforce a sense of the road hierarchy.
- Alongside the **primary** and **secondary streets**, a consistent building line will be applied running parallel to the back of the footpath and helping to unify the different house types. Small variations up to 1m may be applied to add emphasis around key **focal** or **corner** locations to give greater prominence when viewed along the street.
- There will be greater flexibility/variation in building line on **tertiary streets**, to accommodate frontage parking typologies and create more intimate vistas and arrangements suitable to these community led spaces.
- There will also be a common building line alongside the **green corridors** and **perimeter open spaces**, running parallel with the frontage access road and providing a degree of formality to unite the variety of house-types along these edges. This will give a clearly defined edge to the development along its eastern fringe and alongside the internal open spaces.

## Building Heights

- Predominantly 2 storeys with the occasional use of 3 or 2.5 storey units in **focal** locations and to add variation in the roofline alongside open spaces.
- Building heights will generally be quite uniform but with some subtle changes derived from the application of a variety of house-types across plots– reflecting the character of the existing housing development.
- On smaller-scale, more intimate **tertiary streets**, buildings should generally be lower than those on the **primary** and **secondary street**, helping to reflect the street hierarchy. This may include the use of bungalows or units with rooms in the roof. In these areas it is also acceptable to have small clusters of similar house-types (no more than three in a row) to provide a sense of formality.

## Roof Profile

- Generally simple and consistent roof line running parallel to the street.
- Use of facing gables to add variety to the streetscape and turn **corners**.
- Use of roof dormers (to match existing development).
- On **primary** and **secondary streets**, ridge-lines should generally be arranged to run parallel to the street, with occasional punctuation by feature gables. **Corner** plots may also be marked by the gable end facing the higher tier street.
- On **tertiary streets**, the pattern of roof profile should be maintained but be more intimate in scale.

## Frontage Articulation

- Generally little articulation or projection within the frontage, with doors and windows set close to the building line – except for projecting gable features and the flat or arched floating porches that are a feature of the existing development.
- Bay windows may be used on larger properties overlooking the surrounding **open spaces** or **green corridors** and on the gabled ends of properties turning corners.

## Architectural Details

- Generally simple symmetrical or regular arrangements of doors and windows, with upper storey features aligned with those on the lower storey.
- Simple window sill and lintel.
- Use of painted render, arched porches and casement windows on key **feature buildings**.
- Occasional use of timber style detailing on some facing gables – reflecting their use within the existing development and echoing the exposed timber framing seen on some traditional buildings within Penkridge.
- Some half rendered properties, with brick to the ground floor, reflecting examples of traditional housing within Penkridge.
- Banding or contrasting brick used to emphasise the building plinth, reflecting the existing housing in the adjacent development.

## Set Back

- No more than 3 metres on primary or secondary streets to deter the removal of front boundaries for frontage parking and give strong definition to the street.
- Greater flexibility on **tertiary streets** and **private drives** to accommodate a range of housing and parking typologies and provide opportunities for tree planting within front garden spaces (where appropriate).

## Boundary Treatments

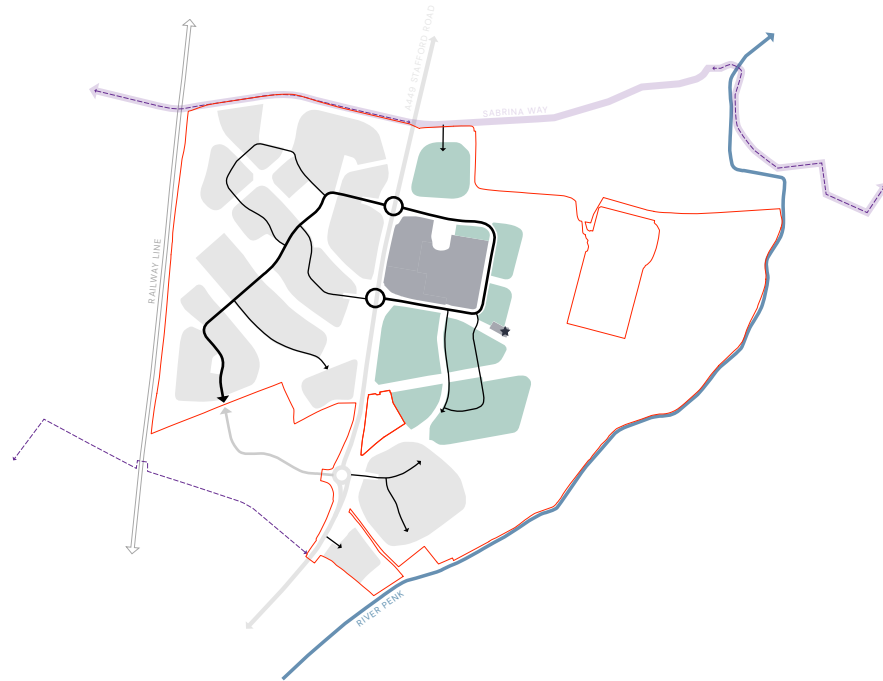
- Black metal railing along **primary** and **secondary streets**, supplemented by a clipped ornamental hedgerow where space allows.
- Black metal railing, grassed margins and ornamental planting to define front gardens on **tertiary streets** and alongside **green corridors** and **open spaces**.
- Hedge, timber fence and/or knee rail to define boundary between **private drive** and **open space/green corridor**.
- High brick walls (materials to match buildings) and/or higher specification fencing to secure rear property boundaries when exposed to the street or public realm.

## Materials

- Typically red brick – tones to match existing development.
- While or cream painted render may be used in some key corner or focal locations, on half rendered properties, or on timber framed gables.
- Exposed timber style frames on some feature gables to match existing development.
- Roofs will be slate (or similar) or mid to dark brown flat clay tiles to match existing development and reflect local vernacular.



# Lower Penkside



## THE VISION

This character area is located to the east of Stafford Road, on gently rising ground between the River Penk and Stafford Road. It has an east facing aspect, with views out towards the river corridor and across to Cannock Chase. There are a number of traditional buildings within the immediate context, including the listed buildings to the north of Lower Drayton Lane, the farm buildings at Lower Drayton Farm and Old Drayton Farm and houses fronting on to the River Penk corridor along Teddesley Road.

It is therefore appropriate for the built character within this area to reflect some of the variation and informality evident within the traditional vernacular of the settlement and surrounding area. While this will typically comprise 2 storey red brick housing with slate or flat clay roof tiles, there may be opportunities to incorporate interpretations of locally distinctive features such as chimney details, fish-scale roof banding and exposed timber framing, in key focal locations.

Buildings should be arranged to positively address the green corridors and open spaces that provide structure to this area, and where possible should be orientated to take advantage of the views eastwards towards the river and Cannock Chase.



Examples of Local Vernacular within Penkridge and the surrounding area



Precedent Images of what the character area might look like



Vignette (1:750)



Parking recessed to the side of properties along secondary street

Smaller units, deeper setback and frontage parking along tertiary street

Housing arranged to positively address vistas along tertiary streets

Variety of larger detached and semi-detached housing along the secondary street united by a consistent building line set back running parallel with the street

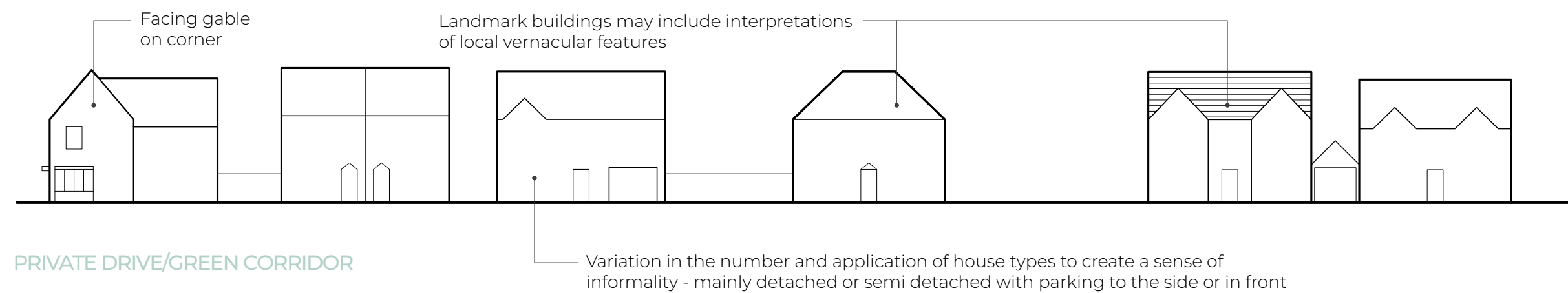
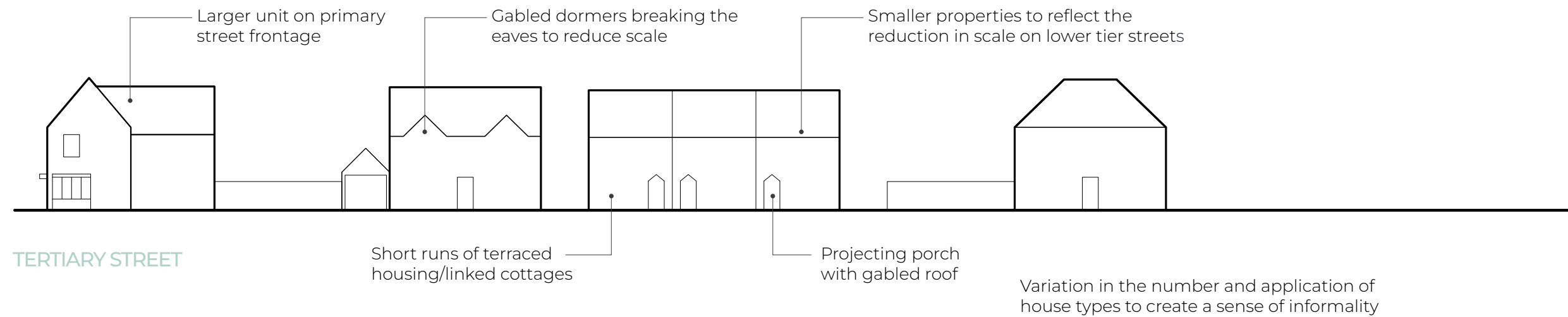
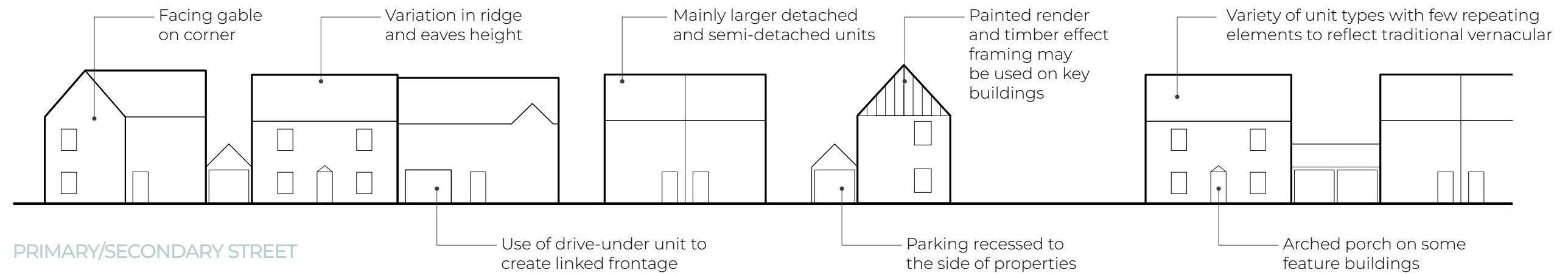
Private drive allows frontage onto green corridors

Green corridor accommodating footpath link and existing hedgerow

Corner units addressing both the secondary street and the green corridor



## Typical Frontage Elevations (1:300)



# LOWER PENKSIDE CODING PRINCIPLES

## Building Form

- Predominantly detached or semi-detached properties with short runs of terraces or linked cottages on lower tier streets.
- In keeping with the traditional vernacular there should be variation in the number and application of house-types across all street typologies to give a sense of **informality** and diversity, with few repeating elements.
- On **primary streets** there will be no parking at the front of properties and on secondary streets it will be discouraged. This will ensure that the frontage has a strong visual relationship with the street. Unit types will mainly be detached or semi-detached properties along these streets so that parking can be recessed on driveways to the side or to the rear. This may include the use of drive under/drive through arrangements with accommodation above to create linked frontages.
- There may be scope for larger building massing and forms around the **community hub** to accommodate flatted or later living accommodation (where required) with parking contained within the perimeter block.
- To reflect the more intimate, small scale character associated with **tertiary streets** there is more scope for short runs of terraced housing, flats over garages (FOGs) or linked cottages alongside frontage parking, either within curtilage, on-street or within small parking court areas.
- **Private drives** will mainly be located alongside open spaces – particularly around the outer edges of the development where densities will be lower to reflect the traditional settlement pattern. Combined with functional restrictions on the number of properties served unit types will tend towards larger detached and semi-detached properties with parking within the curtilage, either to the side or to the front.

## Building Line

- While there will be variation in the appearance and arrangement of house-types within this character area, the building line, in tandem with the parking arrangements, will help to reinforce a sense of the road hierarchy.
- Alongside the **primary** and **secondary streets**, a consistent building line will be applied running parallel to the back of the footpath and helping to unify the different house types. Small variations up to 1m may be applied to add emphasis around **key focal** or **corner** locations to give greater prominence when viewed along the street.
- There will be greater flexibility/variation in the building line on **tertiary streets**, to accommodate frontage parking typologies and create more intimate vistas and arrangements suitable to these community led spaces.
- There will also be flexibility in the building line alongside **green corridors** and **private drives** depending on the context. In more central, urbanised locations a more consistent, common building line should be applied to help unite and formalise the disparate built elements whereas in more rural, outer edge locations a more varied building line will help to add a sense of informality and individuality.

## Building Heights

- Predominantly 2 storeys, with 2.5 to 3 storeys acceptable in **landmark** locations and surrounding the **community hub**.
- In keeping with the more irregular, varied character embodied in the traditional vernacular context, there should be notable variation in ridge and eave heights along the street, almost on a plot by plot basis.
- On smaller-scale, more intimate **tertiary streets**, buildings should generally be lower than those on the **primary** and **secondary street**, helping to reflect the street hierarchy. This may include the use of bungalows or units with rooms in the roof (with gabled dormers breaking the eaves) reflecting local cottage typologies.

## Roof Profile

- Generally simple and consistent roof line running parallel to the street.
- In some places, gabled dormers breaking the eaves may be used to reduce the building scale.
- Facing gables to be used sparingly – mainly on larger properties or **turning corners**.
- On **primary** and **secondary streets**, ridge-lines should generally be arranged to run parallel to the street, with only occasional punctuation by gabled dormers or a facing gable.
- On **tertiary streets**, the variation in roof profile should be maintained but should be more intimate in scale.
- On the outer edges there may be much greater variation in both roof profile and heights to help reinforce the sense of informality. This may include sections of facing gable and gabled dormers.

## Frontage Articulation

- Generally little articulation or projection within the frontage, with doors and windows set close to the building line.
- In some locations projecting porches with pitched/gable roofs (characteristic of the local area) may be appropriate to add variety.
- Bay windows may be used on larger properties in key locations, such as fronting onto the surrounding **open spaces** or **green corridors** and **terminating vistas**.

## Architectural Details

- Generally simple symmetrical arrangements of doors and windows.
- Simple window sill and lintel.
- Additional detailing to buildings in key **focal** or **landmark** locations may include interpretations of vernacular features such as chimneys, timber framing or fish-scale banding roof tiles.
- Use of enclosed projecting pitched/gable roof porches.

## Set Back

- No more than 3 metres on primary or secondary streets to deter the removal of front boundaries for frontage parking and give strong definition to the street.
- Building frontage set to the back of the footpath or with a reduced set back (up to 1 metre) around the **community hub** and for **focal buildings** where a deviation from the consistent building line is desired and appropriate.
- Greater flexibility on **tertiary streets** and **private drives** to accommodate a range of housing and parking typologies, opportunities for tree planting within front garden spaces (where appropriate) and to reinforce the sense of informality around the outer edges.

## Boundary Treatments

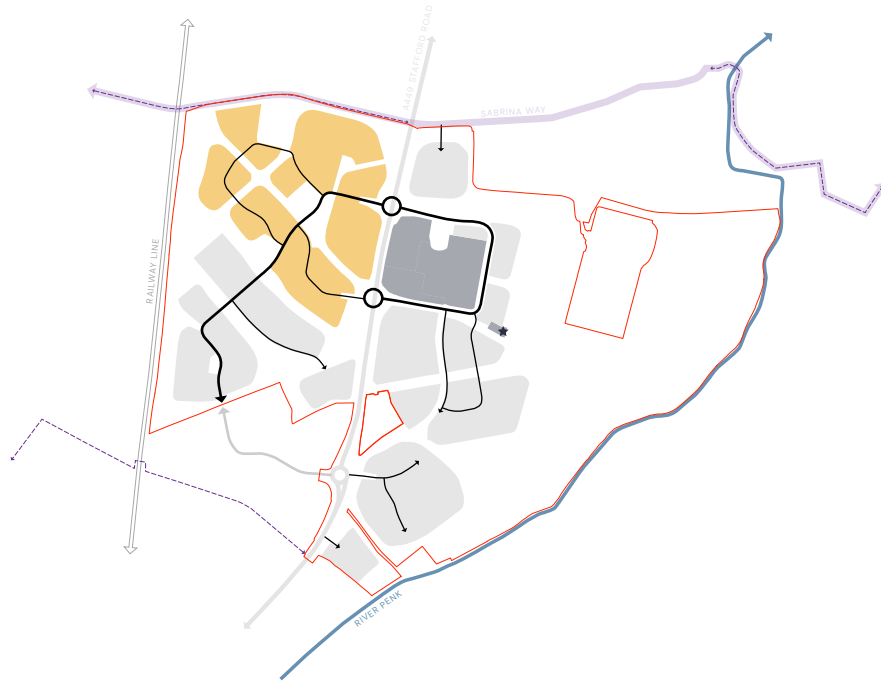
- Low brick wall on **primary** and **secondary streets** to give a strong definition to the property boundary – topped with railing in key focal locations.
- Hedging along **green corridor** and **open space** frontages or on **tertiary streets** where the setback exceeds 1.5 metres.
- Ornamental planting or privacy strips (defined by a surface material change) on **tertiary streets** or **private drives** where the setback is less than 1.5 metres.
- High brick walls to secure rear property boundaries when exposed to the street or public realm.

## Materials

- Mainly red brick – dark and earthy tones to be informed by the traditional local colour palette.
- Timber cladding and /or painted render may be used in some key **corner** or **focal** locations, to add variety to groupings on tertiary streets and alongside green corridors.
- Roofs will be slate (or similar) or mid to dark brown flat clay tiles to reflect local vernacular.



# Upper Penkridge



## THE VISION

This character area is located to the west of Stafford Road in the northern part of the site, on a ridge of higher ground extending northwards. The elevated position allows for views back towards Penkridge and east towards Cannock Chase. There is little existing built context in this area of the site, with the most notable features being the covert and woodland plantations along the northern edge, the brick railway bridge and the storage lagoons. With this in mind there is an opportunity to do something a bit different in this area of the site – still with its roots in the local vernacular but taking a more contemporary approach to some of the building forms and treatments.

Key building groups and landmark buildings will reflect the more rural aesthetic and simple building forms of surrounding farms, appropriate to this area's position on the northern periphery of the site. While many of the basic rules on form, massing and scale for the built environment in this area will have commonality with the wider development, it is envisaged that there are more contemporary approaches to the architectural treatments and materials. This may include the greater use of timber cladding (reflecting the surrounding woodland plantations and the wooded skyline around Cannock Chase to the east), modern brick finishes or colour palettes, and the use of fenestration or terraced areas to take advantage of contextual views.



Examples of more contemporary approaches to built form

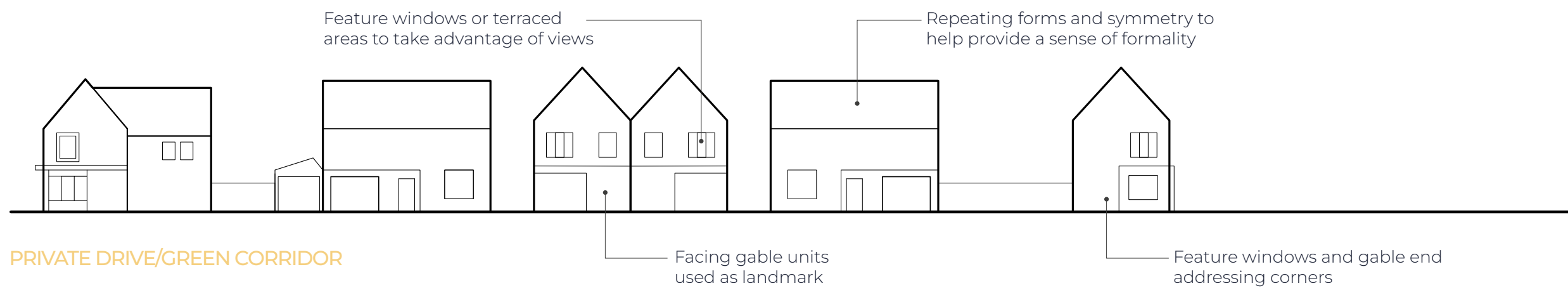
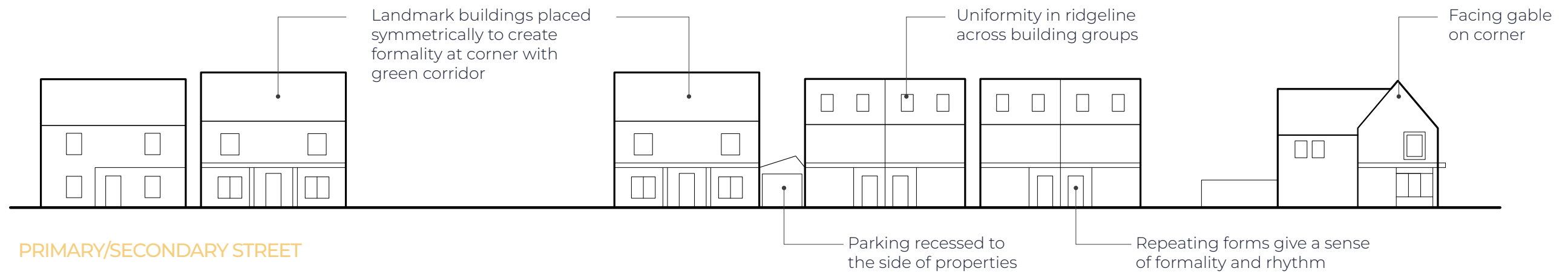


Vignette (1:750)





## Typical Frontage Elevations (1:300)



# UPPER PENKRIDGE CODING PRINCIPLES

## Building Form

- In keeping with the more contemporary approach within this area there should be a greater sense of formality, with a more selective number of house-types across all street typologies, simplified building forms and greater use of repeating elements and symmetry within the layout.
- On **primary streets** there will be no frontage parking and on **secondary streets** it will be discouraged. This will ensure that the frontage has a strong visual relationship with the street. Unit types will mainly be detached or semi-detached properties along these streets so that parking can be recessed on driveways to the side or to the rear. This may include the use of drive under/drive through arrangements with accommodation above to create linked frontages.
- To reflect the more intimate, small scale character associated with **tertiary streets** there is scope for short runs of terraced housing, flats over garages (FOGs) or linked cottages alongside frontage parking, either within curtilage, on-street or within small parking court areas.
- **Private drives** will mainly be located alongside open spaces – particularly around the outer edges of the development where densities will be lower to reflect the traditional settlement pattern. Combined with functional restrictions on the number of properties served, unit types will tend towards larger detached and semi-detached properties with parking within the curtilage, either to the side or to the front.

## Building Line

- Alongside the **primary** and **secondary streets**, a consistent building line will be applied running parallel to the back of the footpath and helping to create a sense of formality. Small variations up to 1m may be applied to add emphasis around key **focal** or **corner** locations to give greater prominence when viewed along the street.
- There will be greater flexibility/variation in building line on **tertiary streets**, to accommodate frontage parking typologies and create more intimate vistas and arrangements suitable to these community led spaces.
- There will also be flexibility in the building line alongside **green corridors** and **private drives** depending on the context. In more central, urbanised locations a more consistent, common building line should be applied to create a sense of formality whereas in more rural, outer edge locations there may be greater variation in the building line to help soften the built form and create areas for frontage parking and tree planting.

## Building Heights

- Predominantly 2 storeys.
- 2.5 to 3 storeys acceptable in **landmark** locations, such as prominent **corners**.
- In keeping with the more regular, contemporary character embodied in this character area, there should be greater uniformity in ridge and eave heights along the street, to give a sense of formality.
- On smaller-scale, more intimate **tertiary streets**, buildings should generally be lower than those on the **primary** and **secondary** streets, helping to reflect the street hierarchy and reflecting the relationship between the principle farm house and surrounding outbuildings. This may include the use of bungalows or units with rooms in the roof.

## Roof Profile

- On **primary** and **secondary** streets, ridge-lines should generally be arranged to run parallel to the street, with only occasional punctuation by gabled dormers or a facing gable. Where these occur, they should be positioned to be symmetrical or as a regular repeating element within the frontage, to create a rhythm and reinforce the sense of formality.
- Common roof profiles should also be applied within **tertiary streets** but will be more intimate in scale. This may include the use of rooflights on the upper storey.
- On the outer edges, repeating elements should help reinforce the sense of formality within this character area. This may include simple contemporary forms based on the traditional gabled end roof profile seen locally or take more bespoke inspiration from surrounding farm buildings. It may also include sections of facing gables or dormers

## Frontage Articulation

- Generally little articulation or projection within the frontage, with doors and windows set close to the building line.
- In some locations projecting porches with gable roofs (characteristic of the local area) may be appropriate to add variety.
- Feature windows or upper floor terraces may be used in some locations, where overlooking surrounding **open spaces** or **green corridors**, to take advantage of views.

## Architectural Details

- Generally simple symmetrical arrangements of doors and windows.
- Simple window sill and lintel.
- Contemporary window styles may be used to add interest.

## Set Back

- No more than 3 metres on **primary** or **secondary streets** to deter the removal of front boundaries for frontage parking and give strong definition to the street.
- Building frontage set to the back of the footpath or with a reduced set back (up to 1 metre) around key **focal** buildings and building clusters where a deviation from the consistent building line is desired and appropriate.
- Greater flexibility on **tertiary streets** and **private drives** to accommodate a range of housing and parking typologies and provide opportunities for tree planting within front garden spaces (where appropriate)

## Boundary Treatments

- Low brick wall on **primary** and **secondary streets** to give a strong definition to the property boundary - topped with railing in key focal locations.
- Clipped ornamental hedging along **green corridor** and **open space** frontages or on **tertiary streets** where the setback exceeds 1.5 metres.
- Ornamental planting or privacy strips (defined by a surface material change) on **tertiary streets** or **private drives** where the setback is less than 1.5 metres.
- High brick walls (materials to match buildings) to secure rear property boundaries when exposed to the street or public realm.

## Materials

- Mainly red brick - tones to be informed by local colour palette.
- Painted render or timber style cladding to be used as an accent in some key **corner** or **focal** locations, to add variety to groupings on tertiary streets and alongside **green corridors**.
- Roofs will be slate (or similar) or mid to dark brown flat clay tiles to reflect local vernacular.



# Stafford Road Corridor



## THE VISION

The treatment of the Stafford Road (A449) will be important to the success of the overall scheme, balancing wider strategic highway functions with the ability for pedestrians and cyclists to safely cross the road corridor and access facilities and homes on both sides of the development.

A new roundabout on the northern edge of the development will serve two important functions – creating a distinctive arrival point on the edge of the settlement and helping to slow traffic on the approach from the north. This will mark an important transition point between the rural hinterland to the north and entry into the urban area. Around the northern roundabout, buildings flanking the approach will be positioned close to the carriageway, creating a strong sense of enclosure.

Due to existing constraints elsewhere alongside the road corridor, there is a relatively short stretch of immediate frontage and this needs to make an impact. A sense of formality will create a sense of arrival within the development and the frontage will take contemporary cues from the existing A449 frontage within the village, including the use of render, double height bay windows, and timber frame detailing.



Examples of primary road frontage



Vignette (1:750)



Landmark building positively addressing roundabout

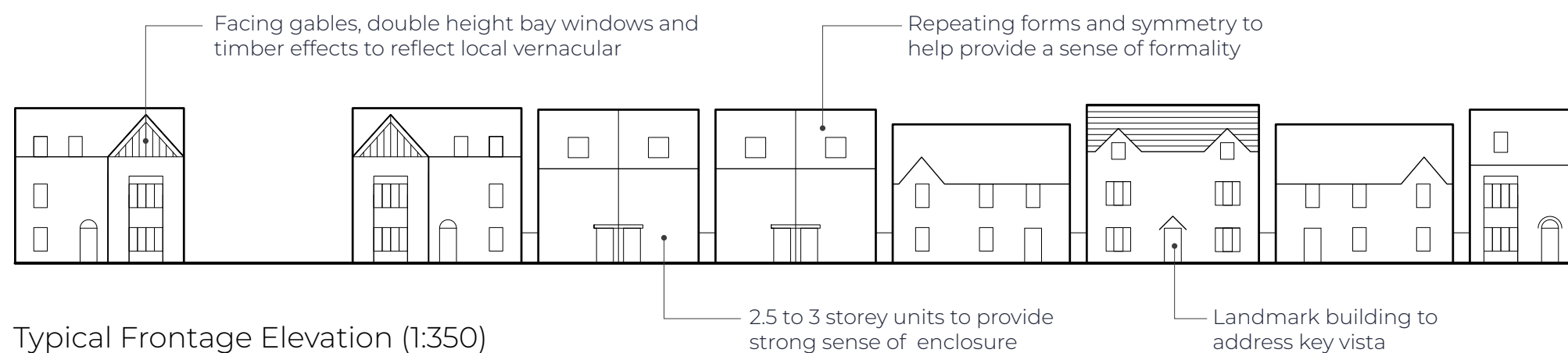
Rear parking courts where direct access to the frontage is restricted

Projections on later living building to break up massing and reflect scale and rhythm of residential housing on the opposite side of the street

Pedestrian connection

Formal tree planting

Closely placed 2.5 to 3 storey housing with minimal set back - providing a strong sense of enclosure to the road corridor



Typical Frontage Elevation (1:350)

## CODING PRINCIPLES

### Building Form

- Closely spaced or linked detached or semi-detached properties or townhouses to create a strong sense of built enclosure alongside the road corridor.
- Formal arrangement with repetitive and symmetrical placement of units to give a strong sense of uniformity.
- Principal elevations, doors and windows facing the street.

### Building Line

- Consistent building line with minor variations - running parallel to the main road.

### Building Heights

- Generally 2.5 to 3 storeys.
- Over 3 storeys may be acceptable in **landmark** locations, such as prominent corners or at the roundabout **gateway**.

### Roof Profile

- Generally simple and consistent roof line running parallel to the street.
- Facing gables may be used to add rhythm but should follow an ordered pattern to reinforce the sense of formality.
- Gabled dormers breaking the eaves may be used for properties over 2 storeys.
- Eaves and roofline should be consistent on both sides of the street.

### Frontage Articulation

- Generally little articulation or projection within the frontage, with doors and windows set close to the building line.
- Where projections are used they should be used in a formal repeating manner to add a regular rhythm. This may include projecting porches or facing gables and double height bay windows (taking its cues from the local vernacular and examples elsewhere along the A449 within Penkridge).

### Architectural Details

- Generally simple symmetrical arrangements of doors and windows.
- Simple window sill and lintel.
- Repeating elements to add rhythm along the frontage.

### Set Back

- Minimal consistent set back (from back of footway) to enhance the sense of enclosure (up to a maximum of 3.0 metres).

### Boundary Treatments

- Low brick wall.
- May be topped by ornamental hedge or railing when applied consistently along the frontage (and on both sides of the street) to maintain the sense of formality.

### Materials

- Typically red brick - tones to be informed by local colour palette.
- Timber detailing on gables and the use of white render may be appropriate in key locations.
- Roofs will be slate (or similar) or mid to dark brown flat clay tiles to reflect local vernacular.



# Green Spine



## THE VISION

The green spines running through the site will be important structural elements within the development, providing direct and attractively landscaped, vehicle-free conduits between the central community hub and the surrounding residential areas, as well as linking to key off-site public rights of way and open space facilities.

These linear open spaces will be enclosed and overlooked by surrounding housing, with the buildings arranged to provide positive frontages along the edges of the green spine. The buildings will have a subtle variation in the architectural treatment to elevate the frontage above other green corridors within the site, emphasising the importance of the green spines within the hierarchy. As well as providing direct pedestrian and cycle links, the corridors will integrate an existing feature tree, new and existing hedgerow boundaries, doorstep amenity space and opportunities for play on the way alongside improved habitat and drainage features.



Examples of green corridor treatments



Vignette (1:750)



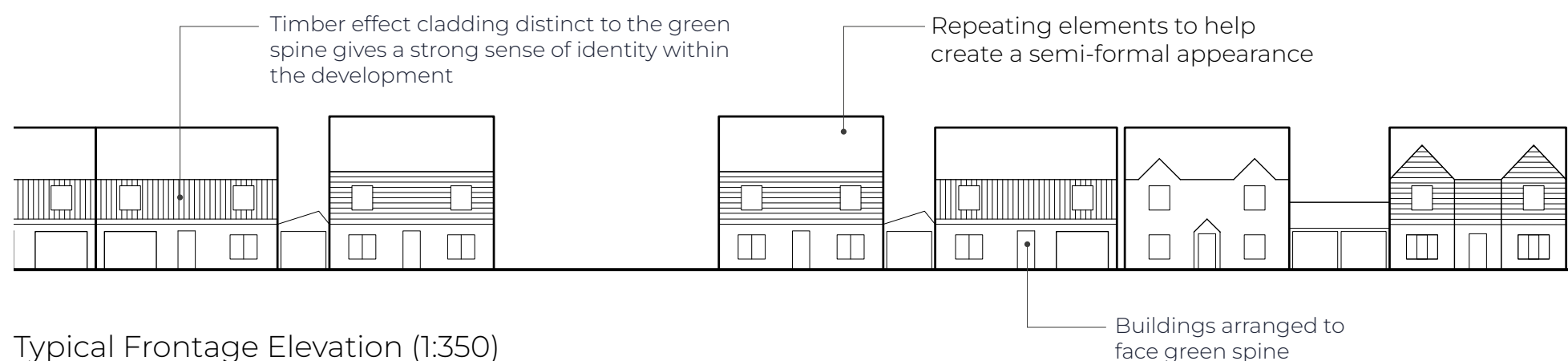
Landmark building positively addressing green spine and linked green spaces

Regular building line with minor variations (except around key focal or corner locations)

Repeating elements to help create a semi-formal appearance

Buildings arranged to face green spine

Local active travel link



Typical Frontage Elevation (1:350)

## CODING PRINCIPLES

### Building Form

- Semi-formal arrangements comprised mainly of detached or semi-detached properties with occasional short runs of terraces, linked cottages or drive-under/through units.
- Principal elevations should face the green space, although with some flexibility around corner locations.

### Building Line

- Regular building line with minor variations.
- Greater variance may be appropriate in some key **focal** or **corner** locations, or to accommodate frontage parking.

### Building Heights

- Predominantly 2 storeys.
- 2.5 to 3 storeys acceptable in landmark locations, such as prominent corners.

### Roof Profile

- Generally simple and consistent roof line running parallel to the street.
- In some places, gabled dormers breaking the eaves may be used to reduce the building scale.
- Facing gables to be used sparingly – mainly on larger properties or turning **corners**.

### Frontage Articulation

- Generally little articulation or projection within the frontage, with doors and windows set close to the building line.
- In some locations projecting porches with gable roofs (characteristic of the local area) may be used to add variety.

### Architectural Details

- Generally simple symmetrical arrangements of doors and windows.
- Simple window sill and lintel.
- Contemporary window styles may be used to add interest.
- Application of a suite of bespoke architectural features that are identifiable with the green spine – to give it a unifying yet distinctive signature. May include porch or window details, cladding, accent materials, door styles, etc.

### Set Back

- Varies depending on parking typology.
- Where parking to the side of property the set back should be no more than 3.0 metres.

### Boundary Treatments

- Low semi-formal hedging.
- Shrub planting where front gardens less than 1.5 metres.
- Estate Railings.

### Materials

- Typically red brick – tones to be informed by local colour palette.
- Timber style cladding to provide a distinctive accent to the green spine – should be uniform colour/treatment to give a strong sense of identity to the green spine throughout the development.
- Roofs will be slate (or similar) or mid to dark brown flat clay tiles to reflect local vernacular.



# Nodes & Landmarks

## THE VISION

**Nodes** are key focal locations where a number of routes or corridors meet. Landmarks are key buildings or structures that have a prominence due to their relative location or position within the development. Due to their highly visible nature, these features have particular value within the scheme for way-marking and for reinforcing the sense of character and identity.

**Landmark buildings** will often have features that help them stand out more from their surroundings, whether through a change in height or material, or through an elevated level of detailing or finish. They will often be located at key entrance or arrival points, on corners, or at the end of vistas, framed by either the street or green spaces.

Some will have strategic importance, helping to signpost key spaces and facilities, such as the community hub, and creating a strong sense of identity along the main approaches into the development and wider settlement – particularly from the north along the A449.

Others will be more localised and will be a product of good quality place-making, with buildings positioned to respond positively to vistas within the street and corner locations. These will be determined by the detailed layouts as they come forward.

The **termination of vistas** along internal streets within the layout should be carefully considered. They command a focal position which should be positively marked by a building unless picking up on a specific view to the surrounding area. Landmark buildings should be designed in such a way that it acknowledges its prominence at the termination of a view. Views should not terminate on non-primary façades, including blank elevations, non-habitable structures (garages or car ports), exposed rear property boundaries or gaps between buildings.

All **corner locations** within the development need to be carefully considered. Buildings in these locations should positively address both aspects by providing active frontage (doors and/or windows) on to the adjacent streets, open spaces or public realm. The primary building frontage should usually face the highest order street typology to reinforce the sense of hierarchy. Driveways and garages should be located to the side or rear of the property so that the building frontage, rather than the parking arrangements, define the corner.

## GATEWAY BUILDINGS



## CODING PRINCIPLES

- Paired building arrangements positioned one on each side of the street and flanking junction or roundabout entrances to form a distinctive 'gateway' feature.
- Building arrangement may be symmetrical or asymmetrical but should have unifying features so they read as a pair – either through materials, architectural detailing or massing.
- Increased massing/height over surrounding buildings to give suitable prominence at key entrance points.
- May include distinctive architectural features that are characteristic of the wider settlement, including substantial, well-detailed chimneys, fish scale banding of roof tiles and decorative timber work on gable ends.
- May also take cues from existing buildings marking the threshold into the village centre after crossing the River Penk, including facing gables and the use of timber frame and white render.
- Will usually be located on a corner and should also adhere to the coding principles for these locations.



## TERMINATING VISTAS



### CODING PRINCIPLES

- Where linear spaces or routes establish a vista, that vista will be terminated by a carefully positioned building or buildings, unless addressing a specific visual feature (such as an existing tree or a view to a surrounding landmark).
- Views should be terminated by the primary frontage and not, for example, a private driveway, garage door, or side boundary wall to a plot.
- Built form terminating vistas can be composed of one dwelling or a series of dwellings but must be designed in such a way that they read as an overall composition and compliment the overall vista.
- Buildings should have features that help them stand out more from their surroundings. This may include distinct but complementary material selections, subtle increases in height, projected and/or cut out elements and contrasting roof profiles.
- Driveways and garages should be positioned to the side or rear of properties to ensure that cars do not disrupt the vista.

## ADDRESSING CORNERS



### CODING PRINCIPLES

- Active frontages should positively address both aspects, through the positioning of entrances, generous windows to habitable rooms, glazed bays/projections and upper level balconies where appropriate.
- Windows to habitable rooms should be present on both the primary and secondary frontage to provide surveillance (bay windows being particularly successful in this regard), and be co-ordinated and ordered as one composition.
- Buildings should have features that help them stand out more from their surroundings. This may include distinct but complementary material selections, subtle increases in height, projected and/or cut out elements and contrasting roof profiles.
- Blank façades will not be acceptable on corners.
- Driveways and garages should be positioned to the side or rear of properties to ensure that cars do not disrupt the corner arrangement.



# Movement & Street Hierarchy

The movement and street hierarchy defines the ways that people will move around the development, but will also fulfil a range of functions – providing active travel routes to encourage people to walk and cycle, community spaces for surrounding residents to meet and access to properties for residents, servicing and emergency vehicles.

Part of making the development legible to visitors and residents will be a easily recognisable street hierarchy and network of active travel routes:

## STREETS

### 1. A449/STAFFORD ROAD

A strategic route running through the site and continuing through Penkridge to the south. It will need to be appropriately designed to maintain functional requirements while allowing people to move freely between both sides of the development.

The initial design is set out at Appendix 1.

### 2. PRIMARY STREET

Within the context of this development this is a residential street that carries local traffic and provides access into the development areas from Stafford Road. Its primary role will be reinforced by the relationship of the built form and the uniform application of a selected number of unit types, materials and architectural details and the inclusion of street trees.

Comprising a 6 to 6.5m wide carriageway with a 2m footway on one side and a 2 to 3m verge for street trees, 3m two-way cycle path and a separate 2m footway on the other.

### 3. SECONDARY STREET

Residential streets with managed traffic flows to prioritise active travel. They provide access to homes, provide a safe residential environment, and support active travel, social interaction and health and well-being.

Comprising a 5.5m carriageway with a 2m footway on each side. Cycling will be accommodated within the highway.

### 4. TERTIARY STREET

These more intimate streets provide access to small groups or clusters of homes. They can be lanes, mews or cul-de-sacs. They should provide a safe residential environment.

Comprising a 5 to 5.5m carriageway with 1.8 to 2m footways on both sides.

### 5. PRIVATE DRIVE

These are short stretches of un-adopted access roads serving a small number of properties, usually on the edge of an open space.

Comprising a 5m shared space.

## ACTIVE TRAVEL ROUTES

### 1. STRATEGIC ACTIVE TRAVEL ROUTE

This will be a key dedicated cycle route linking the existing village centre with the community hub at the centre of the development, following the A449 corridor before diverting through the network of green spaces. It will be an LTN 1/20 compliant route, comprising a 3m wide two-way cycle path and a separate 2m footway.

### 2. SECONDARY ACTIVE TRAVEL ROUTE

These will be secondary active travel routes, associated with the primary road infrastructure serving the development. It will comprise of a dedicated 3m wide two-way cycle path and separate 2m footpaths adjacent to a primary street or linking sections of the A449 corridor.

### 3. LEISURE ROUTE

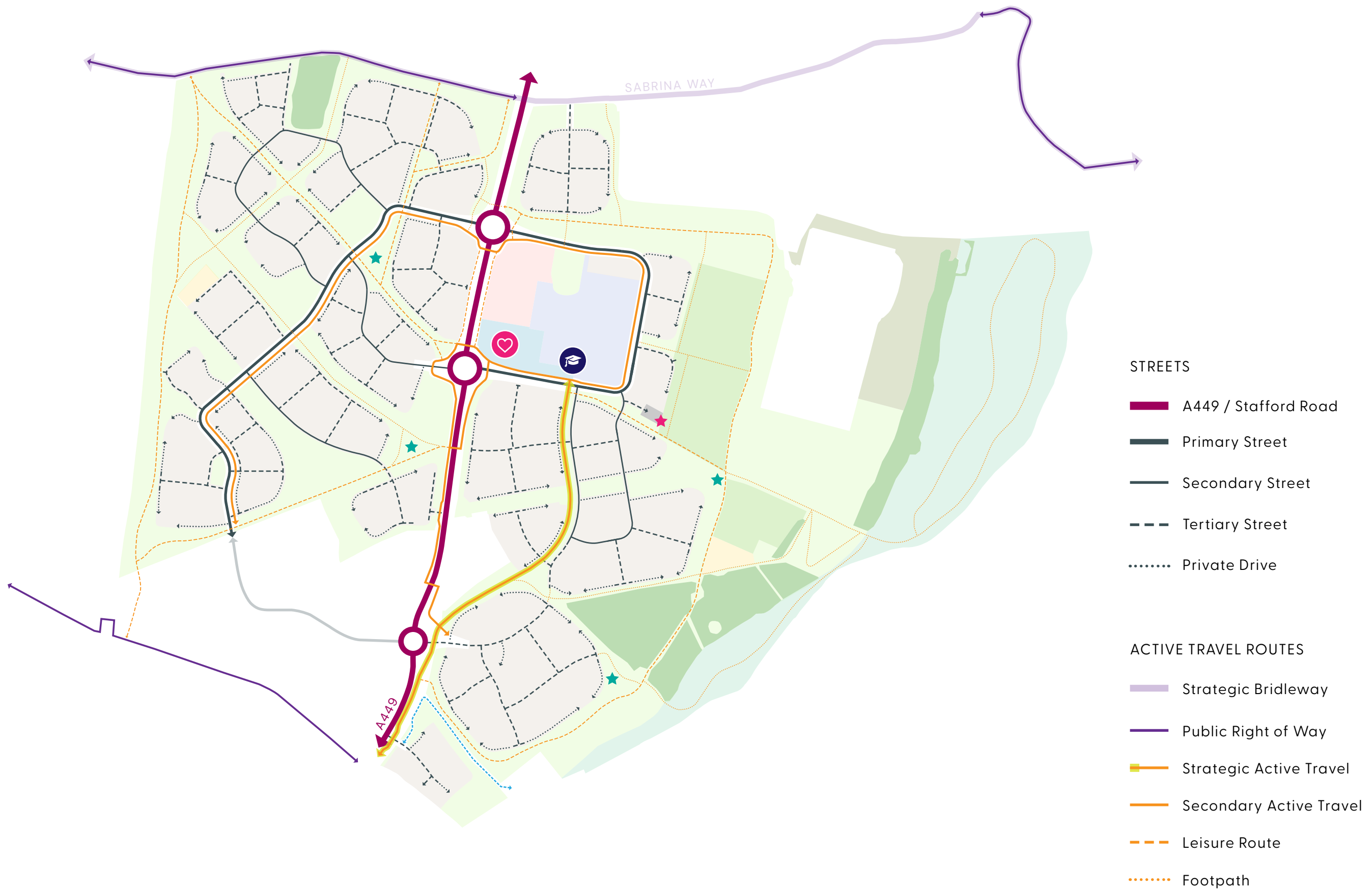
These will be more flexible connecting routes within the development, generally running along the green corridors and within the perimeter green spaces. They will comprise of a 3m wide footpath for shared cycle and pedestrian use, allowing people on bikes or mobility aids to easily access surrounding facilities.

### 4. FOOTPATH

These are simple 2m wide footpaths providing pedestrian access to the surrounding open spaces.

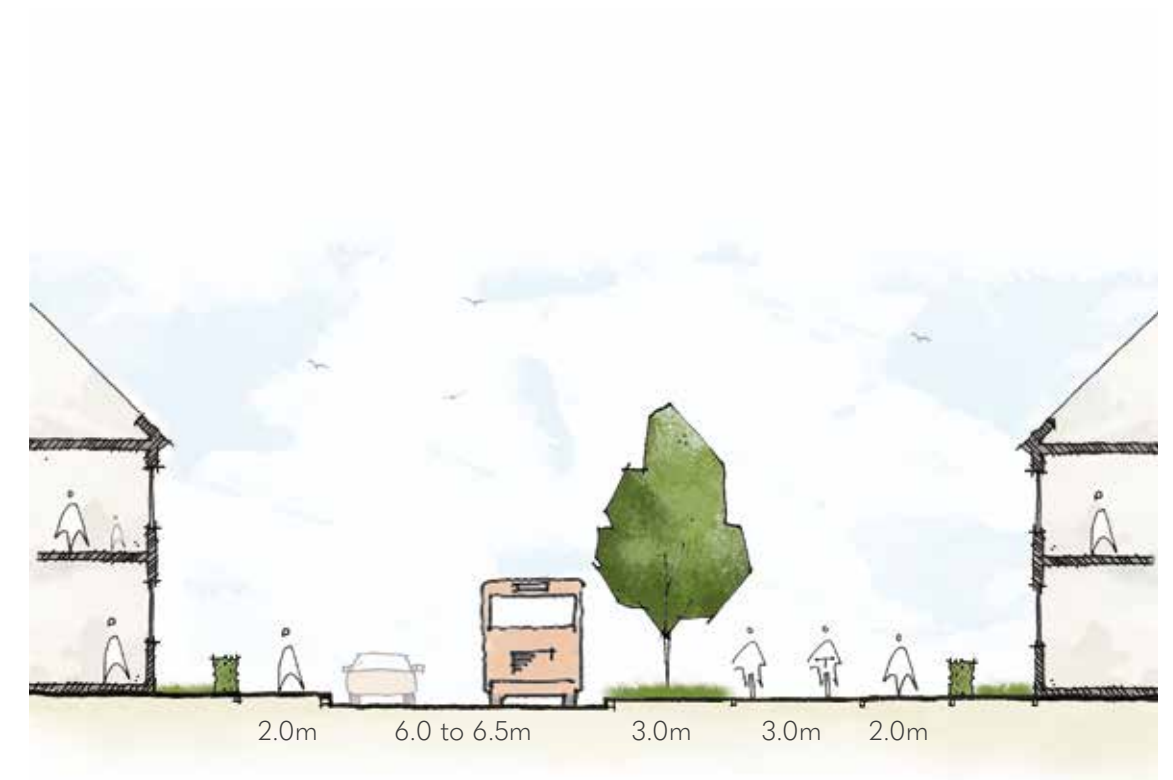
### 5. PUBLIC RIGHT OF WAY

While there are no existing public rights of way within the site, there are a number of routes running alongside, or in close proximity. Links to this existing network will be included within the development.





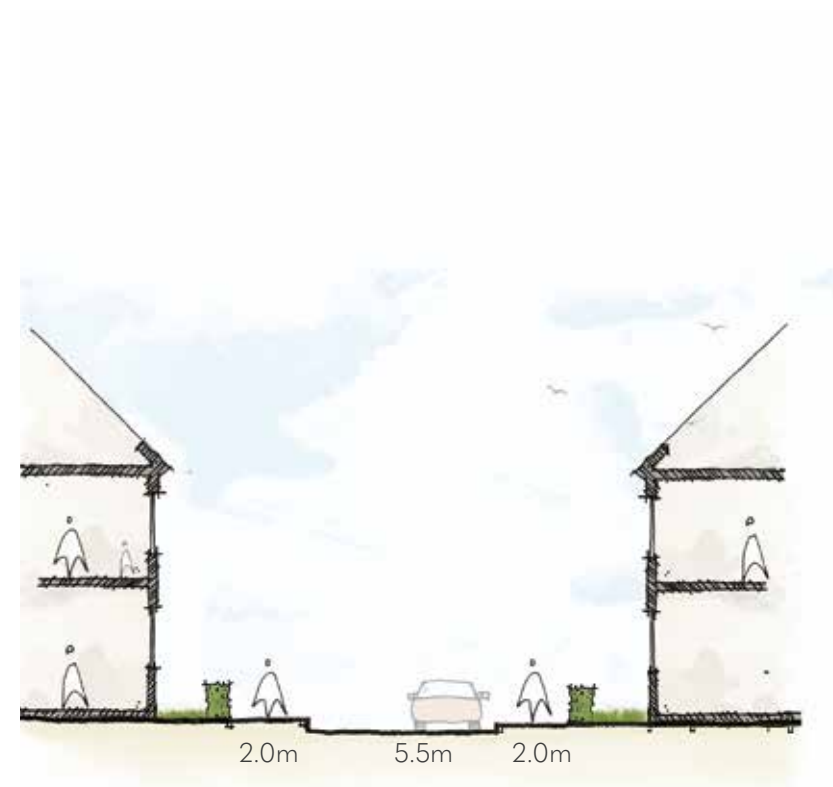
## PRIMARY STREET



### CODING PRINCIPLES

- To adoptable standards.
- Typical carriageway width of 6.0 to 6.5m (to allow for potential future bus use).
- Traffic speed designed for 20 mph.
- 3.0m wide two-way dedicated cycleway, 3.0m verge and separate 2.0m footway on one side.
- 2.0m wide footway on the other side.
- Verge to be sufficient width to accommodate street trees.
- Deliveries and refuse collection from the front of the property.
- Buildings front on to the street and take their main access from it.
- Direct access to properties.

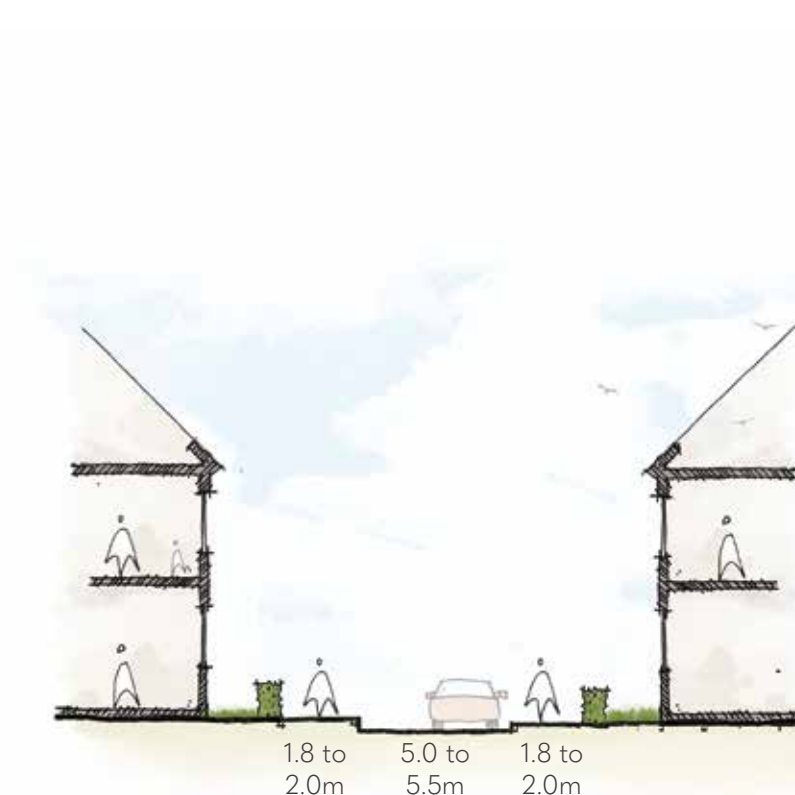
## SECONDARY STREETS



### CODING PRINCIPLES

- To adoptable standards.
- Typical carriageway width of 5.5m.
- Footway on both sides (2.0m).
- Traffic speed designed for 20 mph.
- No grass verge.
- No segregated cycle facilities required due to low traffic speeds.
- Deliveries and refuse collection from the front of the property.
- Buildings front on to the street and take their main access from it.
- Direct access to properties.

## TERTIARY STREETS



### CODING PRINCIPLES

- To adoptable standards.
- Typical carriageway width of 5.0 to 5.5m.
- May include additional width for localised perpendicular parking arrangements.
- Footway of 1.8 to 2.0m width on both sides.
- Traffic speed designed for 15 mph.
- No grass verge – trees set within tree pits/grilles or planted strips to break up frontage parking areas.
- No segregated cycle facilities required due to low traffic speeds.
- Deliveries and refuse collection from the front of the property.
- Buildings front on to the street and take their main access from it.
- Direct access to properties.
- Use of contrasting surface materials to define zones and entrances within the street.

## PRIVATE DRIVES



### CODING PRINCIPLES

- Unadopted.
- Serves a small number of properties.
- Carriageway width varies – typically between 4.5 and 5.0m.
- 5.0m wide at entrance.
- No footway required.
- Traffic speed designed for 5 mph.
- Deliveries and refuse collection from the nearest adoptable road – collection areas to be provided close to private drive entrance..
- Buildings typically front on to the private driveway and take their main access from it.
- Direct access to properties.
- Use of contrasting surface materials to clearly define threshold between private drive and adopted roads.



# Access & Parking Strategy

Car parking has an impact on the quality of the place – not only in terms of how it looks but also in how it is used, particularly by pedestrians. Well-considered parking is convenient, safe and attractive to use. It is also well integrated into the built environment so that it does not dominate the street.

While the masterplan will include walking and cycling routes and access to public transport to try and promote more sustainable travel modes (particularly for journeys under 5 miles) there will still be a need to accommodate private vehicles.

Parking standards will be set out in the local plan to guide the level of parking provision required. Coding will set out how this parking may be designed and accommodated into the development. The typical parking typologies and how they may be applied across the different character areas and street types is set out in the adjacent table and illustrated on the following page.

It is envisaged that the majority of properties will have allocated parking accommodated on plot or within the site, in close proximity to the dwelling it serves. There will also be flexible unallocated parking, for visitors and servicing, that will mainly be within the public highway and designed so as not to cause obstructions for road and footpath users.

	STREET HIERARCHY					KEY SPACE
	STAFFORD ROAD (A449)	PRIMARY STREET	SECONDARY STREET	TERTIARY STREET	PRIVATE DRIVE	COMMUNITY HUB
VEHICULAR ACCESS TO PROPERTY (where required)						
Direct access from the carriageway		•	•	•	•	
Access from side road, rear or service road running parallel to the main road	•	•		•	•	
PARKING TYPOLOGY						
A. Recessed to the side of properties within curtilage	•	•	•	•	•	
B. Undercroft parking/Car port to the side of property	•	•	•	•	•	
C. To the front, within allocated spaces/curtilage.				•	•	
D. Within parking courtyards to the rear.	•	•	•			•
E. On street visitor parking		•	•	•	•	•

## PARKING TYPOLOGY A: PARKING TO THE SIDE

Car parking provided to the side is the preferred parking arrangement for most street typologies as it allows buildings to relate positively and directly with the street without being interrupted by cars.

Car parking spaces must be set behind the building frontage line in order for the approach to work effectively. Where car parking is provided to the side of properties, spaces should be observable from a ground floor window to ensure adequate surveillance.

Driveways should have a minimum width of 3.3m to allow bins/bikes to be manoeuvred past parked cars.



## PARKING TYPOLOGY B: UNDERCROFT/CAR PORT

Essentially this solution provides a variant of 'parking to the side' with the inclusion of accommodation over side spaces to create continuous built form or an open fronted shelter. The parking area should be overlooked by a window in a side elevation.

Parking areas should be a minimum width of 3.3m to allow bins/bikes to be manoeuvred past parked cars. Parking areas can be utilised for access to rear gardens.



## PARKING TYPOLOGY C: PARKING TO THE FRONT

For tighter forms of housing (e.g. terraced housing), car parking spaces provided to the front of the property will be suitable.

Within curtilage, these should be enclosed by well defined front boundaries (such as walls or hedging) to reduce the impact of the vehicles.

When within the street, surface treatments should be co-ordinated with the wider palette of materials and a 0.5m to 1.0m depth defensible planted strip should be provided between the parking spaces and the building frontage. Parking bays should be defined with contrasting sets/studs (white painted lines are not acceptable).

Trees and/or substantial planting are necessary to break up rows of parking (with no more than 4 consecutive parking spaces in a row).



## PARKING TYPOLOGY D: PARKING COURTYARDS

Courtyard parking is not usually considered preferable, however, there will be instances where they are needed to meet parking requirements within the scheme (where access to frontage may be restricted or undesirable)

Individual parking courtyards should be accessed via only one pedestrian/vehicle entrance. A pinch point and change of surface material at the courtyard entrance will clearly demarcate the change of ownership from public to private.

Within the courtyard there should be sufficient lighting and surveillance from neighbouring homes to provide security. Good quality surface treatments (not large expanses of tarmac) should break up the area and clearly define different zones, including routes between homes and their respective parking spaces. There will be no tandem parking arrangements within courtyard spaces.







# GREEN INFRASTRUCTURE CODE





Upper Penkridge

Community Hub

Primary School

Lower Penkside

Drayton Chase

Community Park

River Penk

Railway Line

Stafford Road

Teddlesley Road

M6



# Landscape Strategy

*The Masterplan for the development of the site has been designed to include a network of accessible and interconnected green spaces, which will integrate the development into its setting, deliver a range of benefits for both people and nature, and respond positively to the local context. It will, therefore, enhance landscape character and restore historic landscape elements.*

This part of the DAS provides greater detail about the proposed Green Infrastructure provision, which has been informed by an extensive analysis of the site and its surroundings. It details the proposed strategies in relation to:

- Public open space provision;
- Play provision;
- Enhancing wildlife;
- The sensitive integration of SUDS; and
- Details regarding the proposed tree planting.

The GI provision is comprised of a number of distinct typologies as defined in the Landscape Strategy. Each typology is detailed in the following pages, where key design parameters and principles are set out to inform the detailed design of those green spaces. The intent is to realise the vision as set out in the earlier sections of this DAS by ensuring the delivery of high quality and accessible multi-functional spaces that are well connected and create a true sense of place.





# Green Spine & Corridors



The network of Green Spines and Corridors will physically bind the new neighbourhood together, and also foster a stronger sense of community by encouraging outdoor activities and social interactions. They will, therefore, contribute to a healthier and more sustainable living environment for all.

Focussed along existing and reinstated historic landscape features, they will provide attractive, convenient and safe dedicated active travel routes to connect the housing areas with the proposed community / open space hubs and neighbouring areas.

They will be designed to preserve and enhance the natural spaces between residential areas, framing and creating an attractive setting for the new homes and providing a range of benefits to the new community and environment.

They will provide residents with opportunities for informal play and recreation and deliver direct access to nature from residents' doorsteps; creating a sense of tranquillity and well-being. They will also help to enhance biodiversity and improve air quality.





## CODING PRINCIPLES

- 1 Proposed native hedgerow planted to define corridor and add a biodiversity. To include edible fruiting species to provide opportunities for the community to forage (see Planting Strategy for additional detail).
- 2 Pockets of native wildflower meadow enhance biodiversity across the site and create seasonal interest.
- 3 Play-on-the-way provides opportunities for children to interact with the landscape and add interest and character.
- 4 Mini orchards dispersed within the corridor will provide edible landscapes accessible to everyone and add seasonal interest.
- 5 Community grow boxes placed within amenity spaces providing opportunities for the community to get to know each other and foster a sense of community.
- 6 Groups of native tree and shrub planting provide structure through the corridor and provide a habitat for birds and other wildlife.
- 7 Play Area well connected within green infrastructure network. For details of play areas see Play Strategy.
- 8 Local Active Travel Route- 3m shared cycleway footpath running along the Green Corridor/Spine.
- 9 Footpath connection between housing parcels improves site wide permeability.
- 10 Small social spaces with collective seating (or swings) that engenders discussion. Providing alternative uses in park as described in 'Safer parks, Improving Access for Women and Girls'. To be located frequently along Green Spine/Corridors.
- 11 Amenity attenuation basins to have designed edge that allows for a flat area overlooking the water with a picnic bench and bin.





# Green Spine & Corridors - LEAP

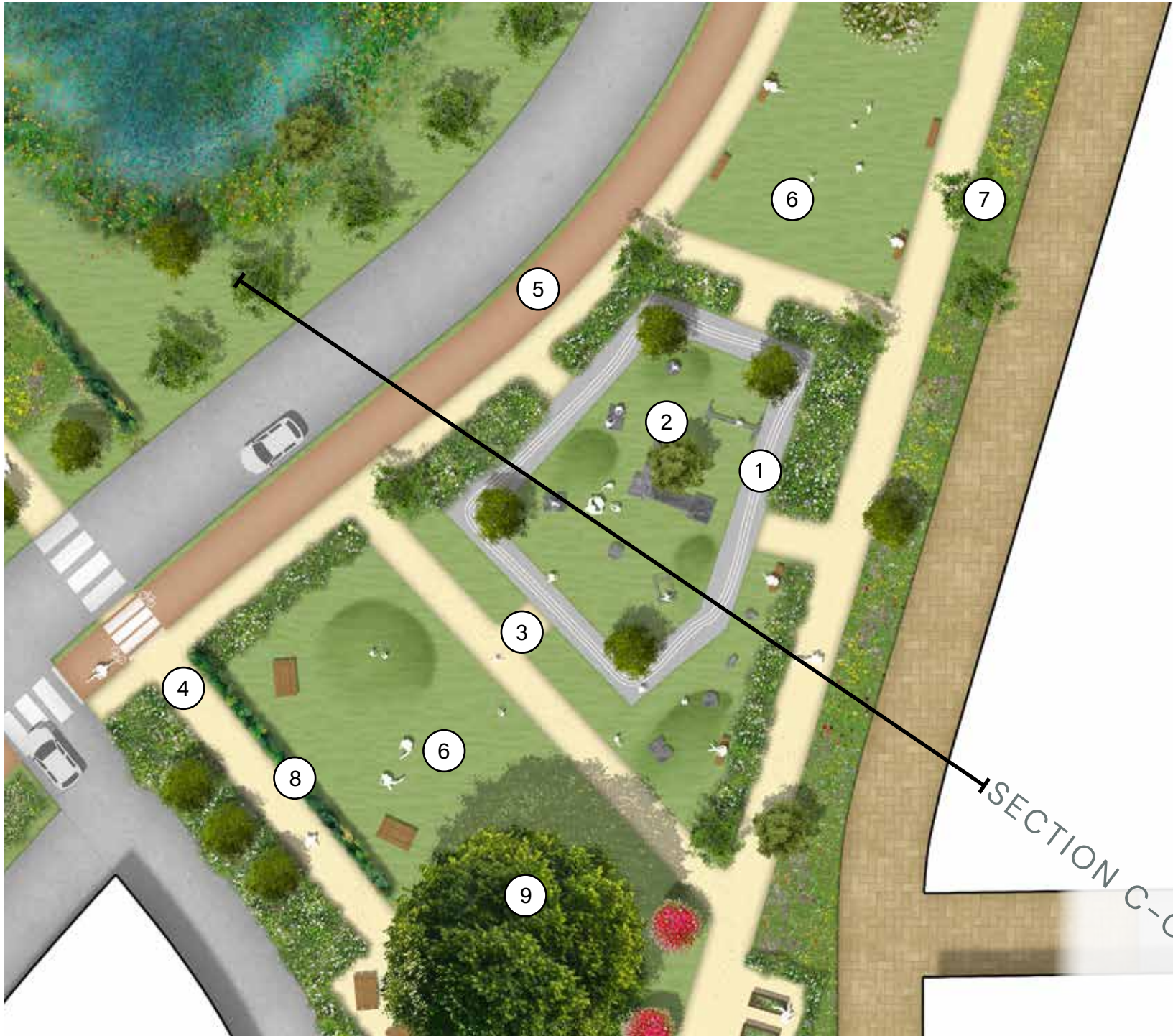


This LEAP is located within the Upper Penkridge Character Area. It is located in a central position adjacent to the Green Spine that connects the western residential areas with the Community Hub.

It is, therefore, proposed that the LEAP has a modern yet inviting theme with planting, a lawn and small areas of hard surface. This will allow children to explore a wider range of play opportunities.







CODING PRINCIPLES

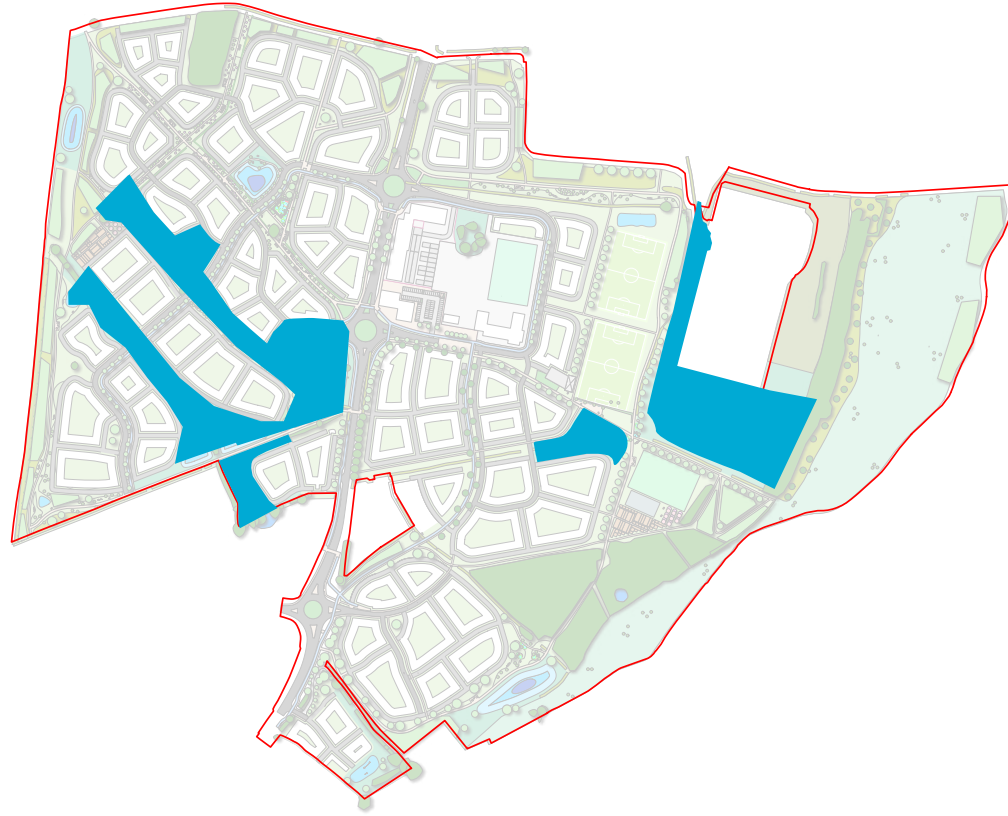
- 1 Loop track allowing for young children to ride bikes and scooters.
- 2 LEAP type play area providing equipment for a range of children. To include smaller area for young children. 400M<sup>2</sup> and 8 pieces of equipment; providing a range of activities.
- 3 Multiple access and exit points and overlooked by houses creating a safe and inclusive environment.
- 4 Located on the green corridor and close to the local active travel route provides easy access from other residential areas.
- 5 3m segregated cycle lane running past the park allows access via key active travel route from wider parts of the scheme.
- 6 Dedicated lawn areas for picnics and informal games.
- 7 Native tree and shrub planting provides structure to the space and shade in the summer months.
- 8 Proposed hedgerow with edible hedgerow species for community foraging (see Planting Strategy for additional detail).
- 9 Existing mature tree retained and used as a key focal point in the space.



SECTION C-C



# Blue Corridors



The Blue Corridors are very similar to the Green Corridors in that they are focussed on existing landscape features within the site. They reflect the existing drainage pattern, working with the underlying topography and incorporating SUDS features (swales and basins) to catch, transfer and discharge surface water across the site.

The blue corridors will also provide a great opportunity for biodiversity enhancement and to accommodate local active travel routes to complement the routes along the Green Spines and Corridors.





## CODING PRINCIPLES

- ① Drainage swale running through the centre of the corridor, conveying surface water and providing opportunities to include a range of native wetland species, providing habitats/diversity.
- ② Existing hedgerows retained and enhanced with additional planting to infill gaps.
- ③ Informal pockets of mown grass, natural play on the way and picnic benches provide places for people to enjoy the space.
- ④ Regularly located benches provide opportunities for people to rest and enjoy their natural surroundings.
- ⑤ 2m wide footpath running alongside swale corridor and between housing parcels creates a more permeable development.
- ⑥ Balancing pond basin created within a widening in the urban form and used as amenity and ecological feature within the landscape (See Drainage Strategy). Areas around edge of pond/basin to be more accessible to public for relaxing and recreation.
- ⑦ Flattened area with distinctive boundary allowing views over pond/basin creating a safe way to interact with wetland wildlife.



SECTION D-D



# Native Edges



The Native Edges are linear green spaces around the edges of the development that will accommodate structural wood planting alongside the existing woodland blocks to soften the visual appearance of the development edge, ensuring the new neighbourhood is sensitively integrated with the surrounding countryside. The proposed woodland will be planted as blocks, which is typical of the landscape character, and will use species which are locally native.

The Native Edges will provide a landscaped buffer between the residential areas and the railway line to the west and strategic bridleway to the north, maintain views to the church, connect ecological habitats and in some areas contain sensitively integrated SUDS.

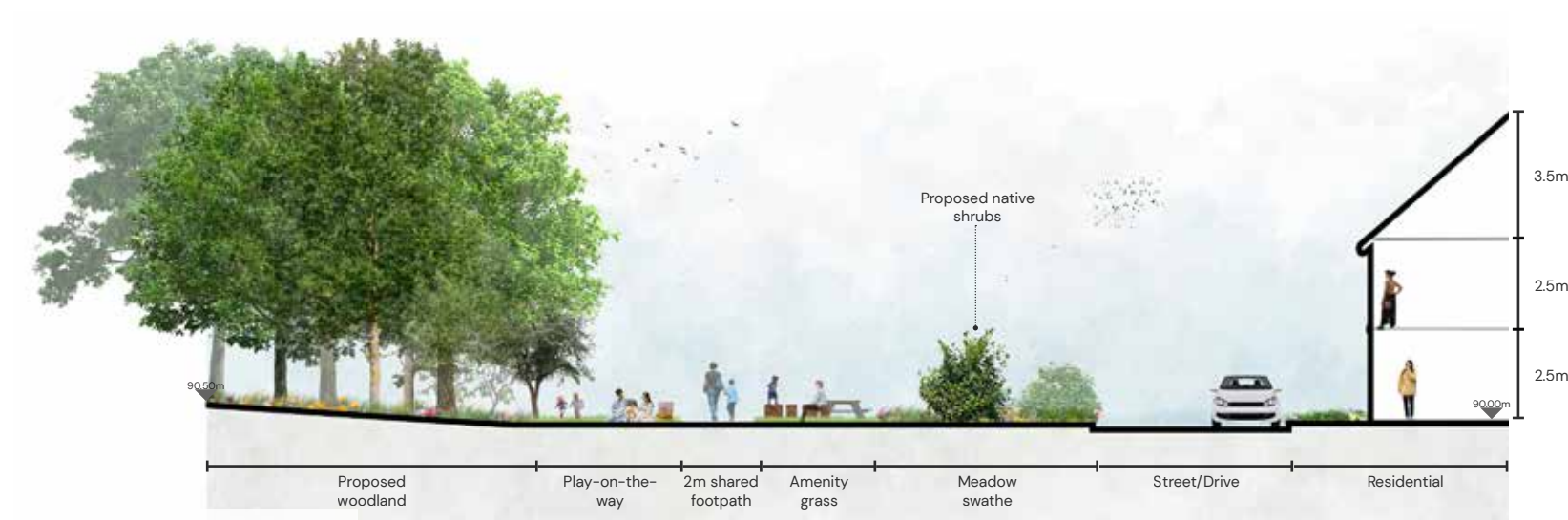
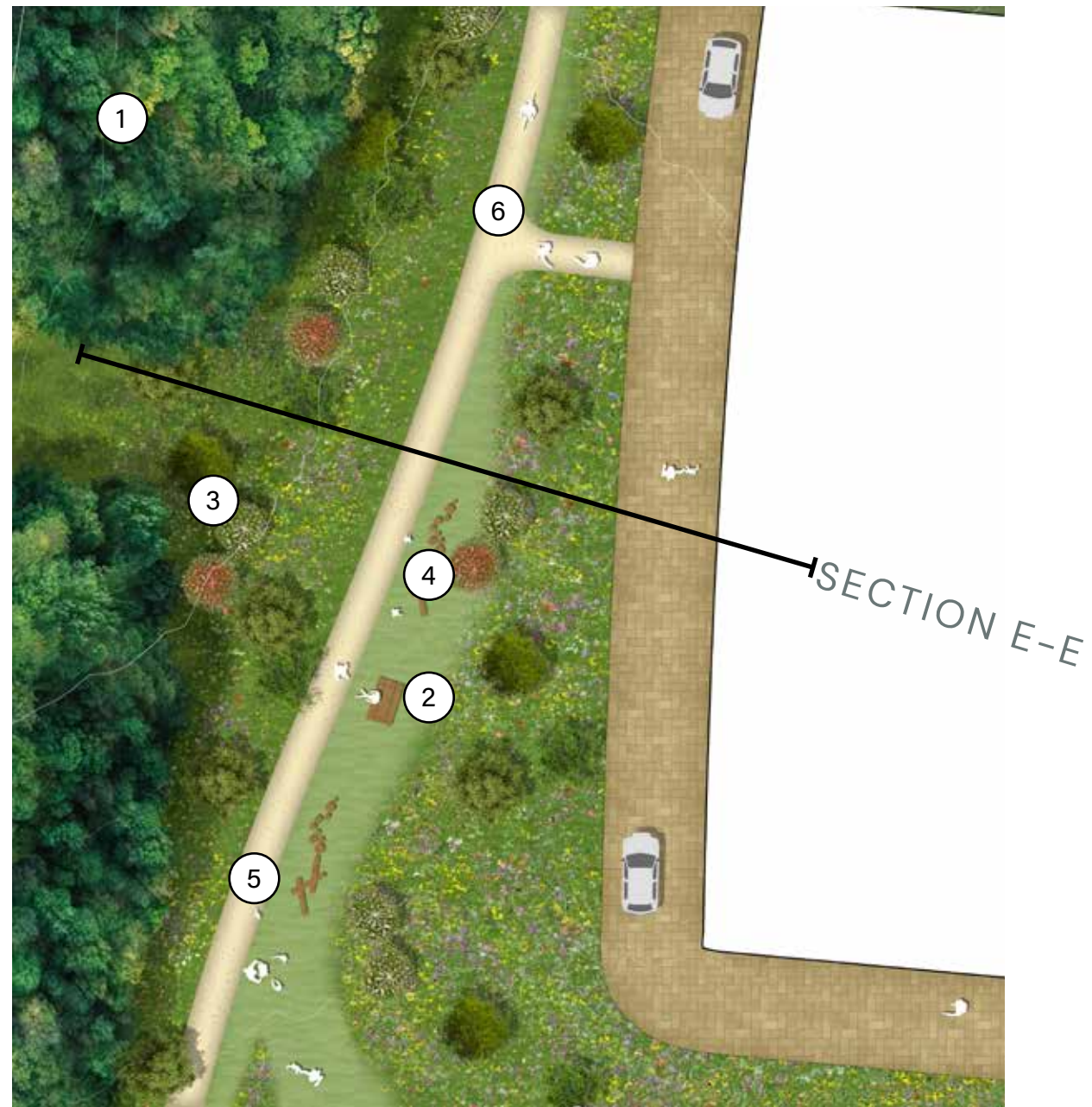
They will also form part of the site wider active travel network, and provide opportunities for informal recreation, natural play on the way and food growing.





## CODING PRINCIPLES

- ① Large areas of woodland planting used to define the site's boundary and provide long continuous wildlife corridors. Any existing hedgerows within boundary to be enhanced
- ② Picnic benches provide areas for the community to spill out into the natural edges of the site and enjoy nature together.
- ③ Informal groups of native under storey planting within wildflower meadow to provide a transitional ecological edge to the native woodland.
- ④ Occasional areas of natural play on the way improve the site wide opportunities for recreation and activate the edge of the residential development. (See Play strategy on pages 78-79 for location)
- ⑤ 2m footpaths around the site provide additional active travel connections



SECTION E-E



# Sports & Recreation



The Sports and Recreation area is located on the Green Spine between the Community Hub and Riverside Community Park. It provides an important facility for the new neighbourhood and will be designed to accommodate a range of sports, recreation and play activities becoming a hub for health and well-being for the surrounding community.

A sports pavilion will be provided that can be easily accessed by the active travel network. This could include facilities such as changing room, toilets and showers. A plaza will be provided here to allow the space to become a meeting area and activity hub for groups such as Couch to 5K or Park Yoga. Parking provision (details to be determined) to serve the sports and recreation area and the community park will also be included.







## CODING PRINCIPLES

- ① 3no Football Pitches and 1 no MUGA provided (to be determined alongside local authority). Grass pitches will be provided with suitable drainage so that all year round activity can be facilitated.
- ② 3m footpath/cycle routes to the sports pitches encouraging active travel to the facilities. Informal routes provided through the space to provide alternative routes for dog walkers etc.
- ③ Dedicated sports pavilion. Parking facility provides vehicular access to the sports facilities and park.
- ④ Meeting area with site furniture for community use.
- ⑤ Native woodland trees and wild flower planting provides a natural edge to the area.
- ⑥ LEAP play area as per the Play Strategy. To be well connected and allowing spill out into the wider environment for play.
- ⑦ Attenuation area provides conveyance for the site wide surface water. Basins to be designed to provide a series different habitats and enhanced biodiversity.
- ⑧ Minimum of 5 pieces of outdoor fitness equipment provided around the space to provide mixed recreational uses for a wider range of people.
- ⑨ Overlooked by adjacent residential areas arranged to face the public space.



SECTION F-F



SECTION G-G



# Riverside Community Park

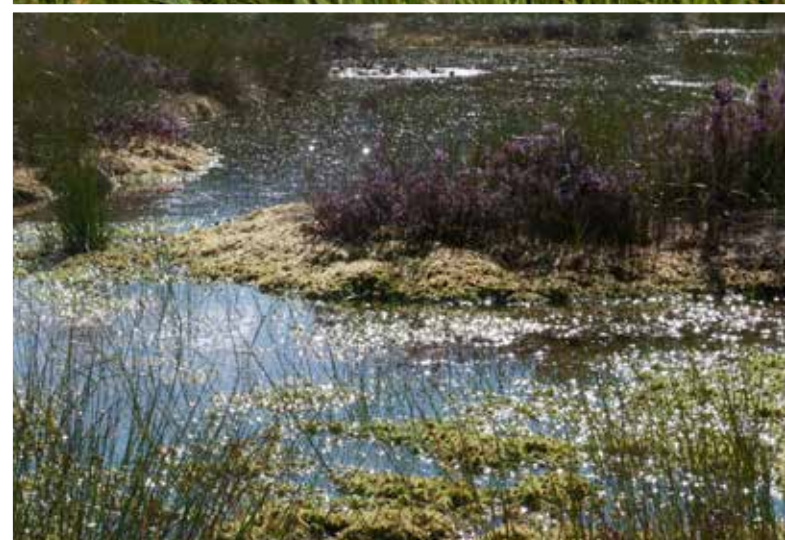


The Riverside Community Park is located in the floodplain alongside the River Penk. It will be a key open public space in the neighbourhood, allowing people to escape into nature and enjoy the riverside experience.

The principal access point will be at the end of the Green Spine linking the Community / Open Space Hubs within the development. A second access will be provided at its southern extent. Circular mown paths will provide opportunities for informal recreation and dog walking.

The landscape will be enhanced with a naturalistic character in line with the recommendations of the heritage assessments. Some tree planting will be provided along the northern and eastern edges to help soften the impact of the M6, whilst maintaining the linear character of the corridor and framing views back towards the village.

Habitat creation and biodiversity enhancement is a key feature of the park. This will be realised through a careful maintenance regime and by gently managing where people may access to the river itself.

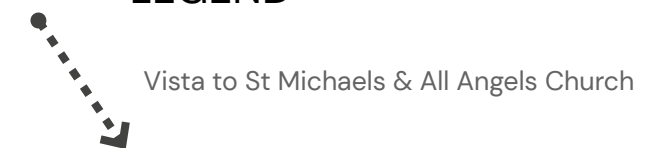




## CODING PRINCIPLES

- ① Large areas of meadow planting allowed to grow within a low maintenance strategy (max 2x a year). Mown grass paths cut through to provide informal footpaths for walking and enjoying nature.
- ② Extend the existing oak trees. Mix of deciduous species (predominantly Oak) to define edge of development and soften the sensitive views from the south.
- ③ New native woodland planting to reduce the presence of the motorway in the parkland.
- ④ River edge improvements as per the ecological strategy. Certain areas improved with bio retention methods.
- ⑤ Picnic seating area aligned where views over local churches are prominent.
- ⑥ Access to river at 2 no. specific points along rivers edge allowing safe interaction for dog walkers and families whilst discouraging use of the other areas of the River Penk allowing to maintain its ecological character.

## LEGEND



SECTION H-H



# Public Open Space Strategy

## OVERALL PUBLIC OPEN SPACE PROVISION

The exact quantum and breakdown of POS will be defined at the detailed design stage, when the final details of the approach to be taken in relation to the built form, landscape scheme, highway design, drainage strategy, etc will be considered in a comprehensive manner.

However, the Strategic Masterplan is ultimately underpinned by a robust masterplanning process, and therefore provides a good general indication of what will be provided.

The Masterplan identifies a generous range of multi-functional POS that significantly exceeds the overall POS requirements of the extant and emerging Local Plans. This is set out below:

- POS requirement for 1,100 dwellings (extant plan); 11.00 hectares;
- POS requirement for 1,100 dwellings (emerging plan): 6.60 hectares;
- Overall POS provision as per the Strategic Masterplan: 21.86 hectares.

## TYOLOGIES:

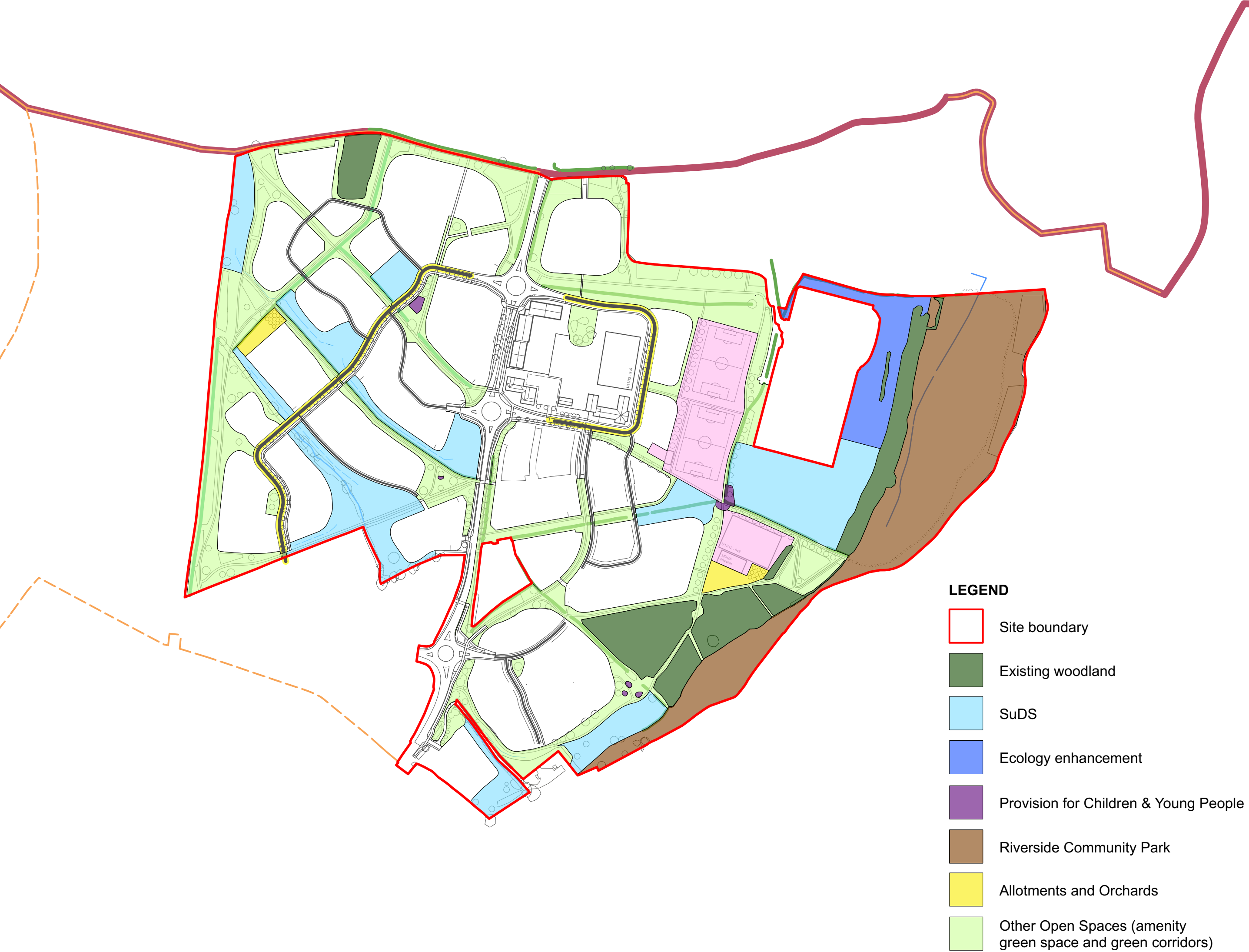
The breakdown in typologies will similarly be subject to refinement through the detailed design scheme, in particular to take account of the landscape strategy.

With that said, the Strategic Masterplan provides an indication of the likely typologies. This again exceeds the POS standards for each typology, as set out in SSC’s Open Space Study Standards Paper.

This is set out in the below table and adjacent plan:

Green and Blue Infrastructure Element (as per Masterplan RevK)		Requirement for 1,100 dwellings (hectares)	Total Area (hectares) as per Masterplan RevK
Play Areas	Provision for Children and Young People	0.11	0.12
Riverside Park	N/A	N/A	7.30
Allotments and Orchards (Community Food Growing)	Allotments	0.33	0.43
	Orchards	N/A	
Other Open Spaces	Amenity greenspace	4.40	14.01
	Green Corridors	2.20	
Total Requirement		7.04	21.86

It is evident, therefore, that the Strategic Masterplan incorporates a very generous quantum of multi-functional POS which will achieve an appropriate breakdown in typologies. In doing so, it will respond to the needs and demands of the new community.








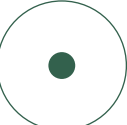



# Play Strategy

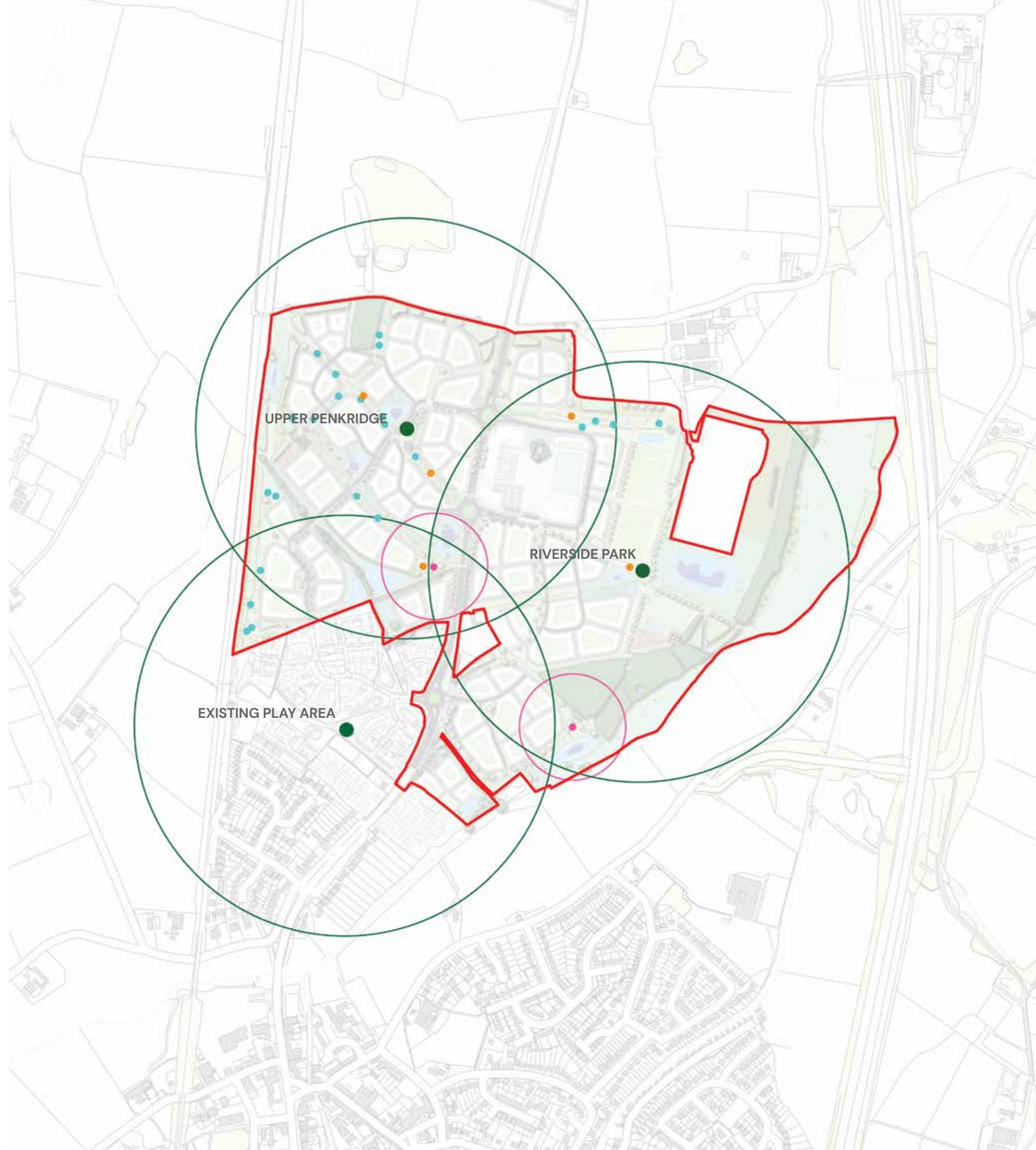
The Play Strategy illustrates that there is a clear and even distribution of formal play facilities across the site. Two LEAPs and two LAPs will be located at key nodes, each with a distinctive design mirroring local surroundings.

The play provision will cater for children of all ages and abilities by offering a range of play equipment. This includes incidental play-on-the-way alongside active travel routes, creating fun and engaging routes for young children, and climbing frames for older children looking to challenge themselves.

In reference to 'Safer Parks: Improving Access for Women and Girls' and 'Making Space for Girls' guidance, alternative spaces for teenagers will also be provided for those who don't wish to use the sports facilities, ensuring there is a safe space for outdoor socialising.

## LEGEND

-  Red line boundary
-  Adjacent development under construction
-  Proposed site
-  LEAP - Local Equipped Area for Play  
(400m, 5 minute walk)
-  LAP - Local Area for Play  
(100m, 1 minute walk)
-  Play on the way & Fitness Trail
-  Social Space designed using guidance;  
Safer Parks Improving Access for Women  
and Girls.





# UPPER PENKRIDGE LEAP

The Upper Penkridge LEAP will reflect the modernistic character of Upper Penkridge. The materials will be more focused on hard surfaces such as concrete, tarmac with painted lines accentuating loop routes and dark timber mirroring the local building renders. A viewing tower would create a focal point on the natural high point.



# RIVER SIDE PARK LEAP

The Riverside Park LEAP will take on a natural, wetland character with a focus on natural play within swales, wetland wildlife and stepping stones mimicking that of a river crossing. This LEAP would use local trees to inspire a willow walk tying into the wetland character of the River Penk.



# LOWER PENKSIDE LAP

This LAP would use the enclosed woodland backdrop to create an intimate woodland themed area. Small play huts allow younger children to engage in group imaginary play whilst outdoor weaving boards and sensory equipment allow for more reflective, solitary play.



# DRAYTON CHASE LAP

The Drayton Chase LAP will take on a more traditional playground character with the use of small timber animals for young children, a subtle link to Lower Drayton Farm and the rich wildlife of Staffordshire.



# SOCIAL SPACE

These areas are designed in line with current best practice to make open space more accessible and feel safe and inviting, especially for girls. By doing this, a more inclusive environment is available to all user groups. This might include group seating, swing seats, fitness equipment and sunken spaces.





# Ecology

The Masterplan has been designed, where possible, to minimise and mitigate the impact of development on habitat loss and fragmentation. Through its delivery of multi-functional green infrastructure, it will also buffer and strengthen existing valued habitats and contribute positively and proportionately to nature's sustained recovery through the phased creation of new linked habitats.

## Creating Effective Links

Significant green links across the site will be maintained through the retention of existing hedgerows and additional tree / scrub planting and creation of species rich / tussocky grassland areas. This will ensure that species can move through the site, and will also provide breeding and foraging opportunities.

## Supporting Biodiversity Targets

The development as a whole will achieve a minimum 10% net gain to biodiversity by creating valuable habitats can be created where there is currently arable land of low ecological value. There are opportunities for the creation of extensive areas of locally important habitats such as ponds, hedgerows, deciduous woodland and species rich neutral meadows.



## Integrating Features

The green space will be designed to integrate the retained existing boundary hedgerows. Existing trees and patches of woodland will be integrated into green corridors and supplemented with additional tree and scrub planting and species rich grassland.

Creation of wooded areas using large canopy native species such as oak, ash, beech and hornbeam will integrate the development into the surrounding countryside. These will be complimented with smaller canopy edge species such as cherry, hawthorn, blackthorn, willow and elder to create transitional zones between the woodland and grassland and provide further habitat variety.

## Securing Implementation

The creation and subsequent management of habitats within the green infrastructure and the relevant biodiversity net gain it achieves will be secured through Section 106 Agreements attached to the grant of the planning permissions.









# SuDS Strategy

Blue Infrastructure forms an integral part of the masterplan proposals, with the development integrating sustainable drainage solutions (SUDS) throughout and, where practical, using green infrastructure elements to control the movement of water within the site, deliver improvements to water quality and bring water closer to people.




The layout of the development responds to the natural landform of the site and seeks to protect and, where possible, enhance features in the existing drainage network. Water will be captured and transferred to outfall to the River Penk by a network of swales and detention basins.

The detailed design of the SUDS features will need to be sensitively considered as part of the future reserved matters applications.

They must be designed to enhance the local landscape character and in naturalistic landscapes should not appear as engineered features with steep sides. All drainage infrastructure (headwalls, spillways etc) should be designed specifically for their location, and standard heavily engineered solutions avoided.

In all cases they will be designed to enhance biodiversity by providing a range of habitats including some permanent water at their base. However, depending on their location in the scheme the basins will be designed to either have an amenity focus with some human access or more of an ecological focus with minimal human access.

## LEGEND

-  Red line boundary
-  Basins with part amenity focus and some human access
-  Basins with focus on ecological requirements and minimum human intervention.





# SUSTAINABLE URBAN DRAINAGE

## AMENITY BASINS

These basins are located closer to the Community Hub, and the Upper Penkridge and Community Park LEAP's. They will be open and accessible with elements of play and walkways encouraging people to use the space.



## ECOLOGICAL BASINS

The ecological basins are located along the blue corridor and within the ecological zone. These will be planted with a range of biodiverse wetland plants, shrubs and trees to increase biodiversity. They will be non-accessible so as to maintain an untouched space for local wildlife.



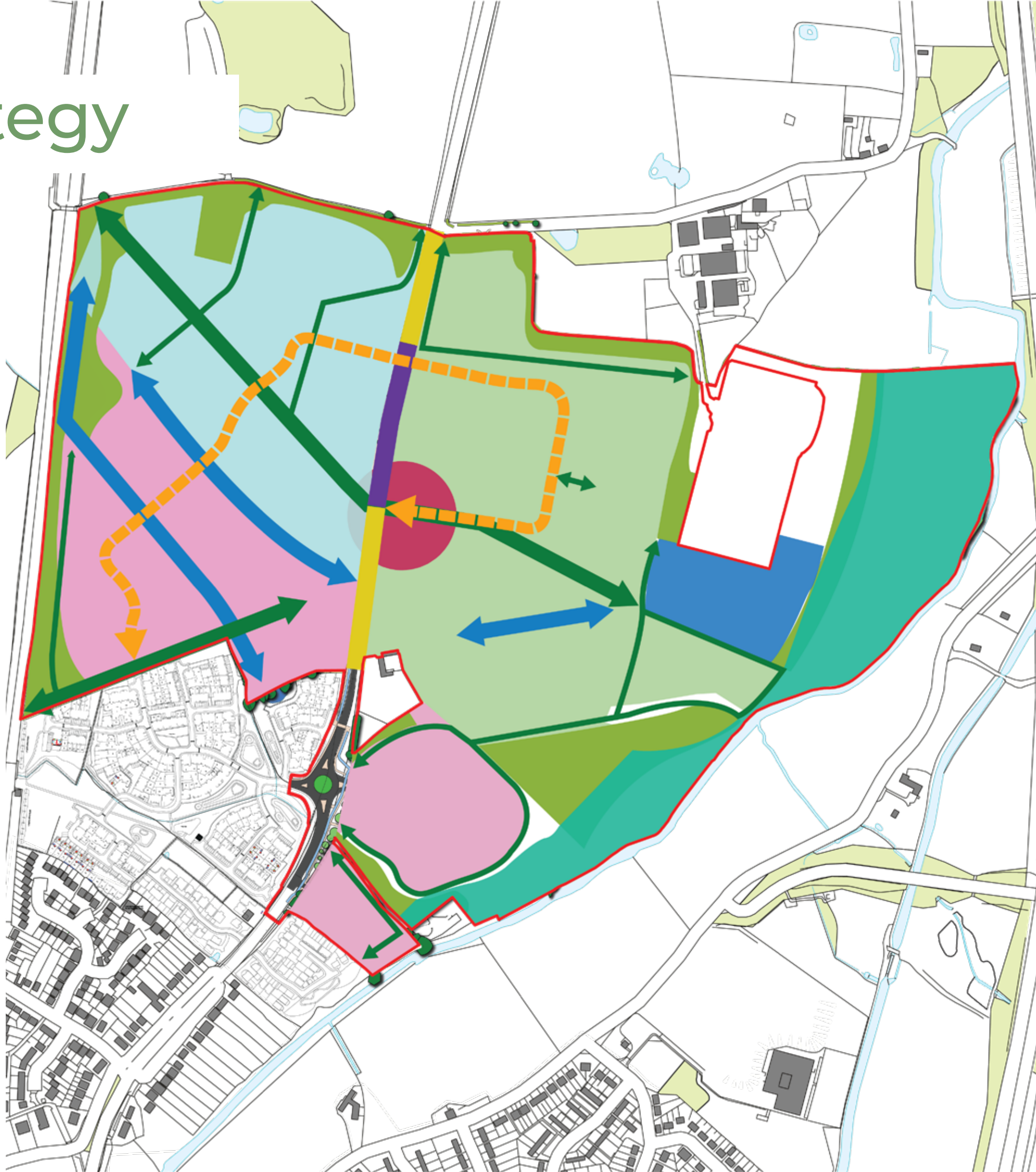


# Planting Strategy


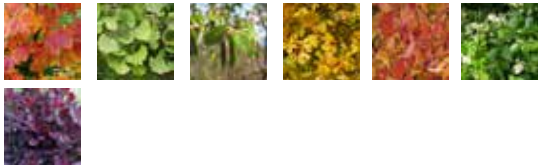


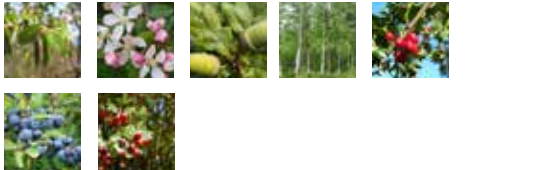

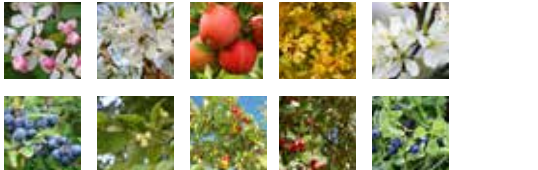



The planting strategy provides a site wide guide that will determine planting characters within the landscape strategy typologies and in focal areas within the scheme.

## PLANTING CHARACTER

- Lower Penkside:**  
Adjacent to the River Penk with a more traditional, historic character, greater use of native species in mixes and tree selection.
- Drayton Chase:**  
A mixture of street trees providing interest in leaf type and colour.
- Upper Penkridge:**  
Ornamental street trees creating a modernistic feel mirroring the contemporary building façades, with formal hedge planting.
- Community Hub:**  
Distinctive public realm trees, providing an attractive setting to the key buildings.
- Stafford Road:**  
Avenue of trees to cover the new residential area to the north, changing the road character and adding to the sense of arrival into Penkridge. The trees will also combine to help reduce the visual appearance of the development from more distant locations.
- Green Spine/Corridor:**  
A variety of native trees, shrubs species and groups of orchard planting running through the site.
- Blue Corridor:**  
Opportunity for wetland shrub and tree species running within SUDs corridors.
- Community park:**  
Wetland landscape. Informal riparian planting. Consistency of large areas of planting here will help to close off views of the development from sensitive locations to the east (including Cannock Chase) as well as helping to conceal the M6.
- Woodland and native edge:**  
Informal groups of native planting that creates a softer edge to the development and helps to assimilate the site into its rural context to the north and west.
- Existing woodland**  
Maintained and reinforced with similar tree species.
- Primary Street**  
Formal tree planting accentuating primary street.





	TREE SPECIES	HEIGHT	GIRTH	NOTES	SHRUBS & HEDGEROWS
	LOWER PENKSIDe <ul style="list-style-type: none"> <li>Malus sylvestris</li> <li>Betula pendula</li> <li>Carpinus betulus</li> <li>Sorbus aucuparia</li> <li>Carpinus betulus</li> <li>Acer campastre</li> </ul>	A range of natural heights and forms	1-18cm girth or naturally feathered specimens to create a wild appearance	Native trees with wild appearance, feathered. Reflecting the wetland character of the River Penk.	<ul style="list-style-type: none"> <li>Crateagus monogyna (native hedge)</li> <li>Corylus avellana</li> <li>Prunus spinosa</li> <li>Cornus sanguinuea</li> <li>Viburnum opulus</li> </ul> 
	DRAYTON CHASE <ul style="list-style-type: none"> <li>Acer platanoids sp.</li> <li>Ginkgo biloba</li> <li>Carpinus betulus</li> <li>Acer campastre</li> </ul>	3-4m	16-18cm	Minimum 2m clear stem;	<ul style="list-style-type: none"> <li>Cornus sanguinea</li> <li>Ligustrum ovalifolium</li> <li>Cotinus coggygia 'Royal Purple'</li> </ul> 
	UPPER PENKRIDGE <ul style="list-style-type: none"> <li>Betula utilis 'jacquemontii'</li> <li>Prunus amanogawa</li> <li>Prunus accolade Malus</li> <li>'John Downie'</li> </ul>	3-4m	16-18cm	Minimum 2m clear stem;	<ul style="list-style-type: none"> <li>Laurus nobilis</li> <li>Carpinus betulus (hedge)</li> <li>Amerlanchier lamarckii</li> </ul> 
	COMMUNITY HUB <ul style="list-style-type: none"> <li>Acer rubrum</li> <li>Ginkgo biloba</li> </ul>	5-6m	20-25cm	Minimum 1.8m clear stem; formal habit.	
	STAFFORD ROAD <div> <ul style="list-style-type: none"> <li>Carpinus betulus 'Frans Fontaine'</li> </ul> </div> <div> <ul style="list-style-type: none"> <li>Malus sylvestris</li> <li>Quercus robur</li> <li>Betula pendula</li> </ul> </div>	A range of heights and forms	8-18cm girth or naturally feathered specimens to create a wild appearance	Native trees with wild appearance, feathered reinforcing the existing hedgerow and native tree species.	<div> <ul style="list-style-type: none"> <li>Illex aquifolium</li> </ul> </div> <div> <ul style="list-style-type: none"> <li>Prunus spinosa</li> <li>Crataegus monogyna</li> </ul> </div> 
	PRIMARY STREET <ul style="list-style-type: none"> <li>Tillia cordata 'Green Spire'</li> <li>Ulmus 'New Horizon'</li> </ul>	5-6m	20-25cm	Minimum 1.8m clear stem; formal habit.	
	GREEN SPINE <ul style="list-style-type: none"> <li>Malus sylvestris</li> <li>Prunus avium</li> <li>Malus domestica sp.</li> <li>Acer campastre</li> <li>Prunus domestica sp.</li> </ul>	A range of heights and forms	8-18cm girth or naturally feathered specimens to create a wild appearance	A range of heights and forms ranging 2-3m feathered.	<ul style="list-style-type: none"> <li>Prunus spinosa</li> <li>Corylusa avellana</li> <li>Cornus mas</li> <li>Crataegus monogyna</li> <li>Vaccinium myrtillus</li> </ul> 
	BLUE CORRIDOR <ul style="list-style-type: none"> <li>Populus tremula</li> <li>Alnus glutinosa</li> <li>Betula pubescens</li> </ul>	A range of heights and forms	8-18cm girth or naturally feathered specimens to create a wild appearance	A range of heights and forms ranging 2-3m feathered.	<ul style="list-style-type: none"> <li>Salix caprea</li> <li>Salix fragilis Cornus 'Midwinter Fire'</li> </ul> 
	COMMUNITY PARK <ul style="list-style-type: none"> <li>Quercus petrea</li> <li>Quercus robur</li> <li>Populus tremula</li> <li>Alnus glutinosa</li> <li>Betula pubescens</li> <li>Salix babylonica</li> </ul>	A range of heights and forms	8-18cm girth or naturally feathered specimens to create a wild appearance	Native trees with wild appearance, feathered. Replicating the wetland character of the River Penk.	<ul style="list-style-type: none"> <li>Crataegus monogyna</li> <li>Corylus avellana</li> <li>Salix caprea</li> <li>Salix fragilis</li> </ul> 
	WOODLAND <ul style="list-style-type: none"> <li>Quercus robur</li> <li>Ulmus glabra</li> <li>Ulmus procera</li> <li>Acer camestre</li> </ul>	3-4m	16-18cm	Minimum 2m clear stem;	<ul style="list-style-type: none"> <li>Corylus avellana</li> </ul> 







# SUSTAINABILITY



# Sustainability

This section highlights the sustainability merits and aspirations of the proposed development. The approach taken has sought to ensure that the proposed scheme incorporates the measures required to respond to key issues in relation to sustainability and climate change, and also seeks to promote inherently sustainable lifestyles for new residents.

## SUSTAINABLE COMMUNITIES

The distinct character established through high quality design, proposed landscape features, and the movement hierarchy will help to create a sense of place, instilling a strong sense of community within the development.

Specifically, the Masterplan's landscape-led approach has resulted in a generous open space / green infrastructure offer. This includes a number of linear and radial green corridors through The Site that will provide substantial new tree planting, ensure that the built form sits within a green setting, and provide direct and attractive leisure routes between the built form and key facilities.

This landscape-led approach will, when combined with the high-quality homes, create an attractive environment with a strong sense of place. It will also establish a sense of ownership for new residents by creating a place where residents can live and meet their recreational needs. The new development will, therefore, promote a high standard of living.

## SUSTAINABLE MOVEMENT

The development will promote sustainable methods of travel through the delivery of a dedicated, LTN 1/20 compliant active travel link between the community hub and village centre. The Masterplan also incorporates radial green corridors that will provide direct links between key uses within The Site and its surrounding area.

The development will also enhance and integrate the existing bus routes along the A449, with new provision for bus stops close to the community hub. It is anticipated that developer contributions will also be made towards increasing the frequency of the services. This will ensure that The Site is well-connected with Penkridge village centre and the additional services and facilities therein, as well as Penkridge Train Station.

The integrated sustainable movement / active travel strategy will, therefore, be beneficial both to existing and new residents.

The development will be supported by a Travel Plan to try and encourage sustainable modes of public transport, including walking and cycling as an alternative to private car use.

## ECOLOGY & BIODIVERSITY

The retention of trees and hedgerows (where possible) and the provision of significant new planting that will more than offset any losses has been a key factor in shaping the development proposals. That, alongside the provision of new green corridors and spaces, will provide screening to the site in views from its surroundings and will create new habitats.

The riverside community park that is to be delivered at the east of The Site, whilst naturalistic in its nature, will support ecological activity associated with the adjacent River Penk and will link that with the open spaces within The Site.

## SUSTAINABLE URBAN DRAINAGE

The Site will largely drain to the east, reflecting the existing topography of The Site. Surface water will drain to existing watercourses within The Site and to the River Penk. SuDS features will be enhanced to provide ecological benefits.

## SUSTAINABLE CONSTRUCTION & ENERGY

Sustainable construction, energy efficiency and fabric first methods will be incorporated into the scheme proposals at the detailed design stage, promoting a high standard of build and construction for the development.

Best practice sustainability will be embraced and the following principles will be incorporated in creating the site layout:

- Optimum plot orientation for solar gain with south facing windows to maximise sunlight.
- Ensuring that buildings can be easily adapted to suit different occupiers needs allowing for the expansion of living areas and storage needs, and where practicable making better use of roof spaces.
- The provision of facilities for refuse, recycling stores, composting and water butts.



- The use of environmentally friendly and sustainable materials, such as locally sourced / recycled materials, aggregates and recycled timber.

Resource / energy efficient buildings will be used, potentially with:

- Efficient heating systems, which might include energy saving micro combined heat and power units.
- Low flow showers, smaller baths and dual low flush toilets as part of controlled water demand and use.
- Low carbon lighting, energy controls and management.
- Double and triple glazing, and improved insulation.

Building Regulations set the standards for most aspects of a building's construction. Part L specifically addresses the requirement for energy conservation and power.

Renewable energy can be incorporated into the scheme, however, pursuing fabric first approaches in the first instance will ensure that all occupiers benefit from reduced electricity bills and will achieve reductions in CO2 production.



# Define.



ST PHILIPS